

STATEMENT OF APPLICANT
1900 Chapman Project Owner, LLC
For 1900 Chapman Avenue
Amendment to STP2012-00112 for Townhomes on Lot 14-B
 April 6, 2015

SITE PLAN Amendment REQUEST:

The Applicant, 1900 Chapman Project Owner, LLC, requests approval of an amendment to Site Plan STP2012-00112 to convert one building of 339 multi-family dwelling units located on the southern Ownership Lot 14B (“**South Apartment Building**”) to 70 city townhouses (“**Townhomes**”). The other approved 319 unit multi-family building (“**North Apartment Building**”) located on the northern Ownership Lot 14A at the corner of Chapman Avenue and Thompson Avenue remains. The general site layout remains generally unchanged and still includes the common driveway connecting Chapman Avenue to Thompson Avenue. The Property is located in the MXTD – Mixed Use Transit District zone at 1900 Chapman Avenue.

The Site Plan Amendment Application proposes:

Use	Description	Approved STP2012-00112	Proposed STP2012-00112A
		658 du total	389 du total
Residential	North Building		
Conventional	Multifamily	319 du	319 du
	South Building		
Conventional	Multifamily	339 du	
	Townhouses		70 du
Commercial	North Building		
	Commercial	5,152 gsf	5,152 gsf
	South Building		
	Commercial	0	0
Cumulative Total		658 du/ 5,152 gsf	389 du/ 5152 gsf

PROPERTY:

The Property is approximately 5.29367 gross acres (230,592.26 SF) known as Lot 14, Block 5, Halpine Subdivision per Plat 24213 with address of 1900 Chapman Avenue. See Plat 24213 attached as **Attachment No. 1**. An ownership Plat was approved for the Property for two ownership lots, 14A and 14B, per PLT2013-00528. **Attachment No. 2**.

The site is the existing Syms retail store bounded by Thompson Avenue (north), the CSX/WMATA railroad right-of-way (east), Montgomery County land improved by Twinbrook Parkway (south), and Chapman Avenue (west). Existing ground level

elevation is highest along Chapman Avenue with a gentle slope downward 8 to 12 feet to the east property line at the railroad tracks. On the Montgomery County fee simple owned parcel to the south, Twinbrook Parkway rises to cross the railroad tracks.

Existing improvements were constructed in the late 1990's and include a two story retail store totaling approximately 70,000 square feet on two identical levels plus 368 paved surface parking spaces. Approximately 80% of the site area is comprised of building or paved parking. Demolition of the existing improvements is imminent.

The Property is zoned MXTD – Mixed Use Transit District and is bounded by: 1) office buildings and service commercial buildings in the MXTD zone to the west across Chapman Avenue and to the north across Thompson Avenue, 2) retail and service commercial buildings in the MXCD zone to the south across Twinbrook Parkway, and 3) diagonally across the WMATA/CSX right-of-way to the north-east is the Twinbrook Metro station and the land for the future mixed use buildings of the Twinbrook Station project in the PD-TC zone. Directly confronting the Property to the east across the WMATA/CSX railroad right- of- way are several office and industrial buildings and land in the Montgomery County CR-2 zone (Commercial-Residential 2 FAR). The closest single family detached house in a residential zone is approximately ¼ mile away on the far side of the WMATA station, the CSX tracks and JBG's Twinbrook Station project at the corner of Halpine Road and Lewis Avenue so it is not in sight or sound of the Property.

The neighborhood along Chapman Avenue is transitioning in 2015 to a commercial-residential district with the opening of JBG's *Galvan* project one block north on Chapman Avenue with 150 multi-family units, and a Safeway grocery store and street-level retail fronting Chapman Avenue (STP2012-00101). Additional residential units have been approved further north on Chapman Avenue in the 1592 Rockville Pike Project (Project Plan PJT2012-00002).

The Property is located approximately 450 feet south of the west entrance to the Twinbrook Metro Station. The Property has approximately 520 feet of frontage on Chapman Avenue, and approximately 360 feet of frontage on Thompson Avenue. Thompson Avenue, a public street, dead ends at the CSX/WMATA right-of-way without a turnaround. An easement along a common driveway through the 1900 Chapman project was approved as part of STP2012-000112 and provides a return route for vehicles from the eastern dead end of Thompson Avenue to Chapman Avenue.

Townhomes Parcel:

Ownership Lot 14B where the townhome community is proposed has approximately 217 feet of frontage along Chapman Avenue and is approximately 444 feet deep. The layout of the townhome community includes two points of vehicular access to the common driveway through the Property with access to both Chapman Avenue and Thompson Avenue.

RELATED ACTIONS:

Pre-Application Meeting PAM2015-0079 for an amendment to STP2012-0012 to convert 339 multi-family dwelling units to 70 townhouse units. Community meeting held October 30, 2014.

Site Plan STP2012-00112, for a mixed use development with 658 multi-family dwelling units in two buildings, 5152 gsf of commercial space, and structured parking, per approval by the Planning Commission by letter dated August 20, 2012. **See Site Plan Approval Letter Attachment No. 3.**

Time Extension - six months approved by the Planning Commission by letter dated May 1, 2014.

Time Extension – six months approved by the Planning Commission by letter dated August 15, 2014

Ownership Plat PLT2013-00528, an ownership plat creating two ownership lots 14A and 14B, per approval by the Planning Commission by letter dated June 21, 2013.

Final Record Plat PLT2011-00511, a corrective plat for one lot recorded May 4, 2011 as Plat No. 24213, correcting Plat No. 20377, per approval by the Planning Commission on April 27, 2011.

Use Permit USE96-00566, a commercial development containing an approximately 70,000 gsf retail building, approved by the Planning Commission on September 18, 1996. The Use Permit was implemented and the retail use continues to present.

Final Record Plat PLT96-00065, creating one lot, approved by the Planning Commission on October 23, 1996. Plat 20377 recorded March 27, 1997.

Use Permit U-436-89, a mixed use development containing 320,800 square feet of office space, 335 multi-family apartments, and 44,200 square feet of retail was approved by the Planning Commission on December 13, 1989. The use permit approval expired.

SCP97-00003, FTPO No. 95-7 with associated Storm Water Management Easement Storm Water Management Inspection and Maintenance Agreement dated March 21, 1997 and recorded in Liber 14769 at folio 059 pertaining to Parts One, Two and Three easement areas, and Tree Conservation Easement and Declaration of Covenants dated March 3, 1997 and recorded in Liber 14769 at folio 0065.

FTP2011-00025. NRI FSD for existing conditions in anticipation of this Site Plan Application for 1900 Chapman Avenue redevelopment approved.

APPLICATION:

Amendment Description:

The Applicant proposes to amend the Project from two multi-family apartment buildings to a project with more diverse housing choices that are not currently available in the southern part of the City's Rockville Pike district. Applicant proposes to convert one of the two approved apartment buildings to an urban ownership community of four-story city townhomes. The Applicant proposes to convert the South Apartment Building on Ownership Lot 14B at the northeast corner of Chapman Avenue and Twinbrook Parkway from 339 one-, two-, and three-bedroom multi-family units to 70 three-bedroom city townhomes. 15% MPDU townhomes are included in the townhome project.

The massing, heights, setbacks, and open space for the proposed layout of the Townhome project is similar to the originally approved South Apartment Building as befits a compact urban townhouse community only one block from the Twinbrook Metro Station. The proposed Townhome community is compatible in scale and massing to the adjoining North Apartment Building to be built under the site plan and the existing and proposed buildings in the mixed use business-residential district being created along Chapman Avenue.

The currently approved South Apartment Building proposed to be replaced on Ownership Lot 14B presented a massing along Chapman Avenue of a continuous six story street wall setback fifteen feet from the sidewalk edge with individual entries from the first floor dwelling units directly to the Chapman Avenue sidewalk. Individual entries from first floor dwelling units likewise exit onto the sidewalk along the common driveway on the north side of the building and onto the private sidewalk along the Twinbrook Parkway frontage from the south side of the building.

Similarly, the proposed townhomes present a massing along Chapman Avenue of twelve (12) four-story townhouses in a group of 5 and a group of 7 creating a continuous street wall setback fifteen feet from the sidewalk edge with individual entries from each townhomes to the Chapman Avenue sidewalk. Individual entries from the townhomes on the south side likewise exit onto the private sidewalk along the Twinbrook Parkway frontage. The entries to the internal townhomes cluster onto private courtyards which connect to the sidewalk along the common driveway. **See Site Plans and Architectural Elevations Attachment No. 4.**

The Project as amended will continue to create a transit-oriented neighborhood oasis on the pedestrian route to the Twinbrook Metro Station entrance and is within walking distance to the goods and services offered in the Rockville Pike Corridor as well as jobs and services in the Twinbrook-Parklawn employment district. The Project is a 3 minute walk to the Twinbrook Metro Station WMATA Redline and within convenient walking distance to the HHS headquarters at the Parklawn Building and future NIAID headquarters on Fishers Lane.

The amendment keeps the signature public use space at the entrance of the North Apartment Building which utilizes innovative architecture as a backdrop at the main

Project entrance on Chapman Avenue with inviting landscaping, public art, bike storage, and community plaza. Grade-level landscaped green areas remain along the perimeter of the Project with paths and sidewalks for walking, exercise, and enjoyment of the outdoors by residents, their families, and pets. **See Landscape Plan, Preliminary FCP Plan, and Approved NRIFSD attached as Attachment No. 5.**

The private on-site access driveway from Chapman Avenue through to Thompson Avenue continues to provide access to the North Apartment Building parking garage and loading area as well as access to the private driveways through the Townhome community. The common driveway has been designed for both functional circulation as well as on-site afforestation and storm water bio-retention areas.

Moderately Priced Dwelling Units:

The Project will comply with the City's MPDU Ordinance and provide 15% of the total units as MPDU units. 15% of the townhomes will be MPDU townhomes. The MPDU townhomes will be three-story three-bedroom units.

Parking and Loading:

Parking for the townhomes is provided in accordance with City Code at 2 parking spaces per three-bedroom unit within two-car parking garages within each townhome. Additional visitor parking is available on-site along the common driveway. The North Apartment Building contains a structured parking facility for its residential and commercial parking needs. Additional publicly available parking is conveniently located on Chapman Avenue, Thompson Avenue, and 5 publicly accessible parking garages within two blocks of the site, including 24/7 parking across Chapman Avenue at 12300 Twinbrook Parkway, 38 metered spaces on the Twinbrook Metro lot one block north and 1097 spaces in the Twinbrook Metro garage.

Truck turning templates have been provided that show that truck deliveries, trash pick-up and emergency vehicle access through the private driveway system in the townhome project is adequately designed. **See On-site Turning Template Attachment No. 6 and Fire Protections Site Plan Phase 2 Attachment No. 7.**

Parking. This Application provides code required parking for its uses on-site.

Loading. Loading for the townhomes is from the private driveways.

Bicycle. The Zoning Ordinance does not require bicycle parking spaces for townhomes. No change is proposed for the North Apartment Building.

Phasing. The Applicant proposes to commence construction on the two ownership lots at different times subject to market conditions. The common driveway will be constructed with the first of construction on either ownership lot.

Trash and Recycling:

All trash and recycling will be collected from individual townhomes by a private contractor for the condominium or homeowners association for the townhome community.

Public Use Space:

The Project provides: 1) a signature public use space featured at the main entrance to the North Apartment Building along Chapman Avenue, 2) landscaped sidewalks and streetscapes along the Chapman Avenue and Thompson Avenue frontages, and 3) a pedestrian path system around the perimeter of the Project and through the Townhome community. The Project provides the required 15% Open Space of which 10% is public use space. See Attachment 8.

The common driveway or “*Paseo*” is the entrance of the signature public use space on Chapman Avenue comprised of a widened streetscape and sidewalk on Chapman Avenue connecting into the private but publicly accessible entrance plaza. Accessible by both residents and the public, the *Paseo* is a landscaped pocket park created by a carve-out of the corner of the North Building and opening up the vista down the landscaped common driveway between the North Apartment Building and the Townhome community. The public use space is activated by the backdrop of a dramatic, six-story glass wall of the North Apartment Building behind which is visible the interior bridges connecting the corridors of the North Apartment Building.

It is anticipated that the public art will be located on both of the ownership lots and will be installed with the construction of each project.

Open Area and Amenities:

The Project provides the required 15 % open space in a mix of landscaped areas, courtyards, recreational facilities, and pedestrian paths.

The proposed Townhome community includes a linear green open space spanning the southern and eastern property perimeter and two linear courtyards which invite residents and guests to stroll and enjoy the outdoors or take a run on the walking paths that connect with the sidewalks along Chapman Avenue and Thompson Avenue and the landscaped sidewalks between the buildings for a complete circuit around and through the Project. The walking circuit provides space for exercise, relaxation, and play for residents, guests, families, and pets.

Accessibility:

The sidewalk system within the townhome project will be ADA accessible.

Conformance of the Site Plan Amendment to the Zoning Ordinance Development Standards of MXTD Zone:

The Project conforms to the development standards of the MXTD zoning classification:

<u>Development Standard</u>	<u>Required/Allowed</u>	<u>Provided</u>
Minimum Lot Size	none	5.29 gross acres (230,583 gsf)
Ownership Lot 14A		2.886 ac (125,728 sf)
Ownership Lot 14B Townhomes		2.407 ac. (104,855 sf)
Minimum Lot Width	10 feet	520 +/- feet
Minimum Open Space	15% (*inclusive)	15 %
*Minimum Public Use Space	10%	10%
Maximum Building Height	120 feet (75 feet at bldg line)	
North Building	120 feet	75 feet
South Townhomes	120 feet	50 feet
Minimum Setback Requirements		
Chapman Avenue	none	10 feet/ 15 feet to TH
Thompson Road	none	10 feet
Parking	<u>533</u> total spaces (<u>386 structured + 140 Townhome garage spaces + 7 space common drive</u>)	
North Building	<u>391</u> spaces (<u>386 structured + 5 surface drive</u>)	
South Building	<u>142</u> spaces (<u>140 garage spaces + 2 surface drive</u>)	

Building Height at Building Line.

The 1989 Rockville Pike Corridor Neighborhood Plan includes functional plans and sections for critical parcels in the Twinbrook Metro Area. The Property is identified as Parcel K and the Plan recommends up to 75 feet at the building line before setting back the building for any additional height along a layback slope. A copy of the function plan and section for the Property is attached as **Attachment No. 9** (page 132 of the Rockville Pike Corridor Neighborhood Plan).

The MXTD zone permits heights above 65 feet at the building line if recommended in a master plan.

The approved building height at the building line is 75 feet for the North Apartment Building and the proposed building height is 50 feet for the Townhome community. This is in conformance with the current adopted Plan.

The Townhome site will be re-graded to increase the elevation of the townhome site along the eastern and south eastern sides of the site. Three-story MPDU townhomes will sit atop a 9 ft retaining wall which is setback from the property line and the CSX tracks to allow a heavily landscape and tree buffer.

Layback Slope

The Layback Slope provided in Section 25.13.05.b.2.d is not applicable to the Property which does not abut any residentially classified land upon which a single family residence or townhouse is constructed. The Property is adjacent to the Metro and CSX railroad right-of-way which is an exemption from the layback slope under 25.13.05.d.(iv). The closest single family house is over ¼ mile away on the opposite side of the WMATA-CSX tracks near the corner of Halpine Road and Lewis Avenue. The closest residential townhouse is in the Cambridge Walk community over 1200 feet from the Property. The Twinbrook Station PD-TC development is located between the Property and any single family house or townhouse.

Shadow Study

No shadow study is required as the Property is surrounded by commercial uses and mixed use zoned land not planned for residential uses.

Conformance with Applicable Master Plans:

The applicable master plans for the subject property are 1) the **Comprehensive Master Plan Approved and Adopted November 12, 2002**, and 2) the **Rockville Pike Corridor Neighborhood Plan adopted April 1989** which includes the Twinbrook Urban Design Guidelines for the Twinbrook Metro Performance District (“RPC Plan”). The update to the Rockville Pike Corridor Neighborhood Plan is under consideration by the Planning Commission and the Mayor and Council but not yet approved and adopted by the Mayor and Council. The Project incorporates the spirit of many of the new urban design concepts being discussed in the updated Rockville Pike Plan.

The Comprehensive Master Plan recommendations for the Rockville Pike Corridor within the Metro Performance District include: 1) *support mixed-use development with a strong residential component*, 2) *improve pedestrian and bicycle circulation routes*, and 3) *encourage redevelopment to improve the image of the Rockville Pike*. The Project is a mixed use development, adds a strong residential component to an existing commercial district, improves pedestrian and bicycle circulation with improvements to Chapman Avenue, and redevelops a large parking lot and single retail use into a modern urban neighborhood.

The Project supports the Comprehensive Master Plan Housing Policies as stated in Chapter Ten to encourage *construction of housing close to the Metro Station*;

encourage multi-family housing in mixed use areas of development; promote policies that encourage *affordable and accessible housing*; and *maintain a mix of ownership and rental opportunities in the City*. The Townhome Project will include 15% MPDUs in three-bedroom townhomes in an ownership community within a block from the Twinbrook Metro station.

The Rockville Pike Corridor Neighborhood Plan includes the Twinbrook Urban Design Guidelines. The Project meets these Guidelines which encourage *orienting retail and services to the street and creating interest at the pedestrian level with landscaped setbacks, public amenities, awnings, plazas, and other devices*. The Project also meets the Guidelines that recommend a continuous build-to line with greater than 50% of the lower floors of both the North Apartment Building and the Townhome Project at the build-to line to create a consistent street edge and provide a positive visual image to pedestrians and motorists.

The Project also supports the goal of the RPC Plan in the Metro Performance District to *encourage residential development where possible; create an integrated public pedestrian pathway system to link the Metro station with residential and mixed use developments; and establish a secondary vehicular circulation system to improve access on Rockville Pike*. The Project improves Chapman Avenue with an enhanced sidewalk and streetscape, a bike path, and creates an improved business district road to serve as a secondary vehicular circulation alternative to Rockville Pike.

With regard to specific RPC Plan recommendations for the Metro Performance District, the Project meets the recommendations to *promote effective use of Metro through land use, promote improved pedestrian circulation, develop open spaces to serve the needs of workers, shoppers and residents, increase residential development within a range of housing types and income, and encourage good building design to contribute to a coordinated urban environment*.

The RPC Plan includes a Functional Plan for the Property which is identified as Parcel K in the Twinbrook Metro Area (p132). The Functional Plan for Parcel K recommends up to 75 feet at the build-to-line along Chapman Avenue before recommending a layback plane for additional heights to 110 feet. See **Attachment No. 9**. The North Apartment Building maintains a building envelope at the build-to line to a height of 72-74 feet along Chapman Avenue. The Townhome development maintains a streetwall of 50 feet of height for the majority of the remainder of the Chapman Avenue frontage. The Project exceeds the minimum 50% continuous street edge recommended in the RPC Plan while breaking up the building massing with access ways into the interior of the site which extends the pedestrian circulation in the area.

The pending amendment to the RPC Plan, ***Rockville's Pike***, is not yet adopted and is a changing document as it moves through the public process. The Applicant has been monitoring the public comment and the proposed Townhome Amendment has been designed to meet the spirit of the Corridor Planning Principles of the pending master plan in the following manner.

Liveable, desirable environment enhanced by thoughtful urban design. The Project as amended with the urban townhome community adds to the creation of a new neighborhood in the Chapman Avenue district and distinguishes Rockville from the nearby Pike and Rose development. The Project includes a transforming and pleasant public realm along Chapman Avenue and Thompson Avenue and internal open space that integrates trees, paths, gathering spaces, and public art.

Multimodal Transportation. The project provides improvements to Chapman Avenue addressing safe and accessible pedestrian and biking infrastructure. The Townhome community adds to the safety and ambience of the pedestrian experience along Chapman Avenue.

Economic Vitality. Adding new residents to support key retail development in the Corridor and providing residents that can walk to the new grocery store on Chapman Avenue in the Galvan Project one block north.

Sidewalks/Streets:

As previously approved in STP2012-00112, the street and sidewalk cross-sections proposed for Chapman Avenue are in accord with the street cross-sections and dimensions approved as part of the Twinbrook Commons Preliminary Development Plan and Detailed Site Plans and as proposed in the pending update to the Rockville Pike Plan.

The Chapman Avenue street section remains the same in front of the Townhome frontage. The private driveways through the Townhome community are 18-20 foot wide, one lane in each direction.

Transportation:

A Traffic Statement prepared by Street Traffic Studies, Ltd. dated March 27, 2015 for the proposed amendment updates the CTR Transportation Report dated November 21, 2011 which was analyzed as part of the STP2012-00112 approval.

The STS Traffic Statement concludes that after trip credit for the existing retail use eliminated on the Property, the amendment of the South Apartment Building will result in a reduction of trips generated by the Project by -100 trips in the AM Peak Hour, -101 trips in the PM Peak Hour, and -91 in the Saturday Peak Hour. Therefore, STS concludes that no mitigation measures are required for the development of the North Building or the Townhome community. A copy of the STS Transportation Statement is attached as **Attachment No. 10.**

A truck turning template exhibits demonstrating that loading, trash, emergency vehicle, and truck access can be accommodated has been submitted as Attachments No. 5 and 6.

Stormwater Management:

The Property is subject to a recorded Storm Water Management Easement and Storm Water Management Inspection and Maintenance Agreement dated March 21, 1997 recorded in Liber 14769 at folio 059. The existing Storm Water Management Easement has been approved to be abandoned and released by the Mayor and Council and replaced with a new Storm Water Management Easement consistent with the approved Storm Water Management Plan and permits for the proposed redevelopment.

A storm water management concept plan for STP2012-00112 has been approved by letter dated December 19, 2011. A Storm Water Management Plan package has been submitted with this Amendment Application.

Adequate Public Facilities:

The Project must satisfy the Adequate Public Facilities Standards found in Rockville City Code Chapter 25.20.01.

Transportation. The Traffic Statement prepared by Street Traffic Studies, LTD, dated March 27, 2015 which updates the CTR Report for the proposed Townhome amendment concludes that the Project will meet the standards for Transportation. **See Attachment No. 11.**

Schools. The Project as amended continues to meet the school test because the Townhome amendment will reduce the total number of students generated by the Project compared to the school capacity reserved to the Project at the time of site plan approval. The number of students generated by the proposed 70 Townhome community is 24 total students K-12. This is significantly less than the 40 K-12 students calculated and reserved as part of the original site plan approval for the 339 multi-family dwelling units in the South Apartment Building which the Townhomes will replace.

The Project lies within the Walter Johnson School Cluster and students are currently assigned to Walter Johnson High School, Tilden Middle School and Farmland Elementary School. At the time of site plan approval, a finding was made that the Project did not cause projected enrollments to exceed **110%** program capacity at any of the assigned schools. Under the City Code, school capacity was reserved for the total number of students calculated to be generated by the site plan approved density.

The following chart compares the number of students for each level projected to be generated for the 339 du South Apartment Building under the rates in place at the time of Site Plan approval in 2012 against those of the proposed 70 Townhome amendment under current student generation rates as follows:

<u>School Level</u>	<u>Reserved STP2012-00112</u>	<u>Projected TH Amendment</u>
Walter Johnson HS	12 students (339x 0.033 factor)	7 (70 x 0.099 factor)
Tilden Middle	14 students (339x 0.039 factor)	5 (70 x 0.072 factor)
Farmland Elementary	14 students (339x 0.042 factor)	12 (70 x 0.166 factor)

Total K-12

40 students

24 students

The proposed Townhome amendment will not have an adverse impact on the adequacy of the school facilities and will reduce the number of students that the Project will generate to the assigned schools.

Fire & Emergency Service Protection. The City's APFS adopted by Resolution October 28, 2013 indicates that the Project site has response from at least 3 separate stations to arrive at the location within 10 minutes. A copy of Appendix D to the APFS is attached as **Attachment No. 12.**

The closest fire station is No. 23 located at 121 Rollins Avenue, just over 1000 feet from the Property.

A Fire access plan has been submitted for review by City Inspection Services staff.

Water/Sewer. The Property is currently served by City of Rockville public water and sewer. An updated Water and Sewer Authorization for the amended use is included in the Application. A copy of WSSC 1 form is attached as **Attachment No. 13.**

The North Apartment Buildings and Townhomes will be sprinklered. Fire flow test results dated October 27, 2011 were submitted December 2011 as part of the original site plan analysis.

A variable width public utility easement is recorded along Chapman Avenue per Plat 20377. Supplemental public utility easement was proposed along Chapman Avenue as part of STP2012-00112. No public utility easement is required along the southern boundary line which abuts fee simple land owned by Montgomery County MD and not Twinbrook Parkway right-of-way.

Landscaping, Screening & Lighting Manual

The Application is designed to meet the requirements of the City Landscaping, Screening & Lighting Manual. The Property will meet the minimum 15% tree canopy.

Transportation Noise

A Noise Analysis was prepared by Phoenix Noise & Vibration, LLC dated November 30, 2011 on the noise impact from Twinbrook Parkway and the WMATA/CSX railway and the proposed mixed use development.

Interior noise levels can be maintained at the maximum 45 dBA Ldn for residential units through modifications to standard building construction. Specific modifications to impacted units will be established during a Phase II Noise Analysis of the architectural plans and building specifications before building permit.

The Planning Commission approved a waiver of the Environmental Guidelines with regard to the open space and public use space as part of STP2012-00112.

Forest Conservation

A Natural Resource Inventory Forest Stand Delineation Plan no. FTP2011-00025 was approved for the Property dated October 13, 2011.

A Preliminary Forest Conservation Plan for the area of disturbance conforming to the Landscape plan for the Project under STP2012-00112 was approved by the City. Final Forest Conservation Plan approval will be obtained prior to issuance of Public Works permits to implement the site plan.

A Preliminary Forest Conservation Plan for the amended Townhome parcel has been filed with this application.

The existing Tree Conservation Easement and Declaration of Covenants in Liber 14769 at folio 065 will need to be abandoned and released and/or modified and replaced with a Tree Conservation Easement and Declaration of Covenants conforming to the new Forest Conservation Plan.

Green Building Regulations:

The Project new construction, including the proposed townhomes, is designed to comply with the Green Building Regulations in Rockville Code Chapter 5 Building & Building Regulations, Art. XIV.

The preliminary LEED checklist approved as part of the original STP2012-00122 indicates that the Project will meet the requirements of the City Green Building Regulations with regard to points attained in specific LEED categories. A copy of the original checklist is attached as **Attachment No. 14.**

The townhome community will participate in the Energy STAR program. The final points for the townhome community will be developed prior to building permit but will be no less points in total than previously approved for the Project.

Art in Private Development:

The North Apartment Building and the proposed Townhome community under this amendment Application will each separately comply with Chapter 4 of the City Code (Publicly Accessible Art in Private Development) ordinance requirements.

Historic Significance Review.

The Project proposes the demolition of the existing Syms store which was previously reviewed by the City's Historic District Commission at the time of the original application and found to have no historical significance meeting the qualifications for preservation.

ATTACHMENTS

1. Record Plat 24213
2. Approved Ownership Plat PLT2013-00528 for Ownership Lots 14A, 14B
3. Site Plan STP2012-00112 Approval Letter dated August 20, 2012
4. Site Plan Sheets C-1, C-5, C-7, C-8, C-9, Architectural Elevations A4.01
5. Landscape Plan L1.01, L1.02, L2.01, Overall FCP (2 sheets), Aprvd NRIFSD
6. On-site Turning Template E-1, E1.1
7. Fire Protection Site Plan – Phase 2
8. Open Space Exhibit
9. 1989 Rockville Pike Corridor Plan p132 Function Plan Parcel K
10. Transportation Report 3/27/15 Street Traffic Studies, Ltd.
11. APFS Appendix D Emergency Response Time Exhibit
12. Water and Sewer Authorization Application TH Amendment
13. Original LEED Point chart STP2012-00112

NO.	CHORD	PERCENT	ARC	ANGLE	BEARING	CHORD
1	57'25.41"	30.00	105.87'	114°	N 07°19'24" E	53.00
2	57'25.41"	30.00	105.87'	65.65°	N 07°19'24" E	53.00
3	57'25.41"	30.00	105.87'	40.13°	N 07°19'24" E	53.00

AREA TABULATION
 Ownership Lot 14-A 125,728 Square Feet or 2.88632 Acres
 Ownership Lot 14-B 104,855 Square Feet or 2.40714 Acres
 Total by this plat 230,583 Square Feet or 5.29346 Acres

PLAT NO.

NOTES:

- 1) Grid values shown hereon are ground surface datum and were derived from WSSC Control Stations 19530, 19532 and 19533.
- 2) This ownership plat is not intended to show every matter affecting the ownership and use, nor every matter restricting the ownership and use, of this property. The ownership plat is not intended to replace an examination of title or to depict or note all matters affecting title.
- 3) The entire property shown hereon is subject to a Tree Conservation Easement and Declaration of Covenants, recorded among the Land Records of Montgomery County, Maryland in Liber 14769 # F-60 63, as amended.
- 4) The property shown hereon is subject to a Notice of Amended Supplemental Final Order and Judgment, recorded among the Land Records of Montgomery County, Maryland in Liber 35967 # Folio 54.
- 5) The property shown hereon is subject to a Declaration and Agreement, recorded among the Land Records of Montgomery County, Maryland in Liber 1798 # Folio 20.
- 6) The Temporary Public Access Easement and Sidewalk Maintenance Easement (SME) established by Plat 24213 and Stormwater Management Easement Part One, Two and Three created in Liber 14769 # Folio 59, shall be released per authorization of Mayor and Council of Rockville October 14, 2013 in Meeting 29-13, by written termination instruments to be recorded among the Land Records of Montgomery County, Maryland.
- 7) The property shown hereon is subject to a Declaration and Grant of a Public Access Easement to be recorded among the Land Records of Montgomery County, Maryland.
- 8) Development of the property shown on this plat will require additional assessments to be established and recorded in the Land Records of Montgomery County, Maryland, including stormwater management easements and public access easements (PAE) prior to building permit.
- 9) This property appears on Montgomery County Tax Map GG 563, Tax Map Grid GG 63.
- 10) This property shown hereon is zoned MXTD.

OWNER'S DEDICATION

We, the undersigned, owners of the property described hereon certify that: The Ownership Plat is prepared pursuant to Section 25-21.13 of the City of Rockville Zoning Ordinance. That the land designated hereon shall not constitute a record lot or plan of subdivision; that all improvements on the ownership lots shown hereon shall at all times be in accordance with the approved Site Plan STP201200112, 1900 Chapman Avenue, as may be amended or superseded, and duly issued by the City of Rockville, and on file at the City of Rockville Planning Division.

Further, the property covered by this ownership plat is subject to an Ownership Plat Declaration of Covenants, Easements and Restrictions executed by the undersigned and recorded among the Land Records of Montgomery County, Maryland concurrently.

As owners of this subdivision, we, our successors, agents and assigns will cause corner markers and any other required monumentation to be set by a Registered Maryland Surveyor.

There are no recorded subs, adjoins all law, liens, leases, mortgages or trusts affecting the property included in this ownership plat.

1900 CHAPMAN PROJECT OWNER, LLC, a Delaware limited liability company

By: Hines 1900 Chapman LLC, a Delaware limited liability company, its administrative member

By: Hines 1900 Chapman Associates Limited Partnership, a Texas limited partnership, its sole member

By: Hines interests Limited Partnership, a Delaware limited partnership, its general partner

By: Hines Holdings, Inc., a Texas corporation, its general partner

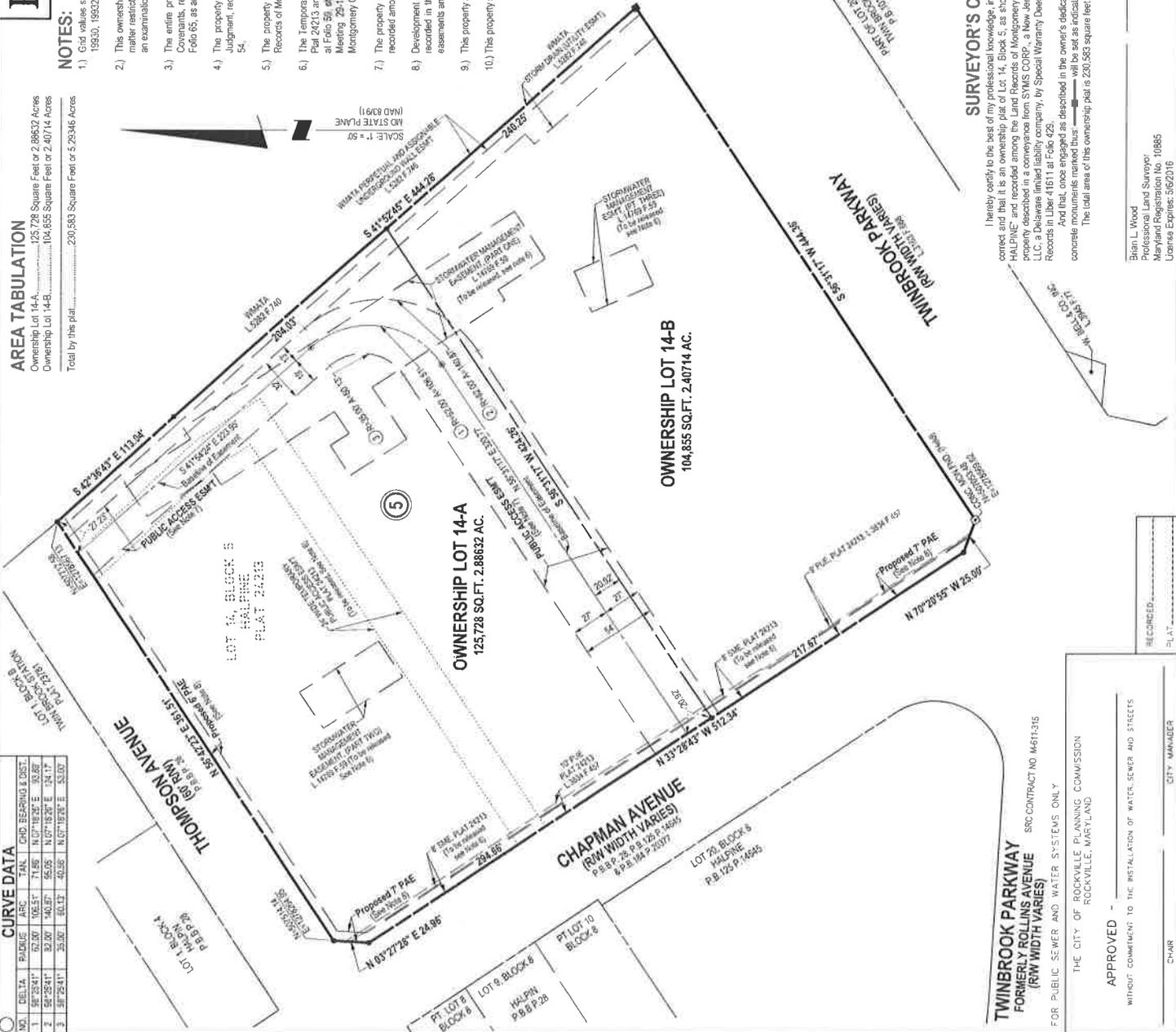
Charles K. Walters, Jr., Senior Managing Director
 Date _____ Witness/Attest _____



OWNERSHIP PLAT
LOT 14-A AND 14-B, BLOCK 5
HALPINE
 (AN OWNERSHIP PLAT OF
 LOT 14, BLOCK 5, HALPINE
 PLAT 24213)
 CITY OF ROCKVILLE
 ROCKVILLE (4TH) ELECTION DISTRICT
 MONTGOMERY COUNTY, MARYLAND
 SCALE: 1" = 50' DATE: JANUARY, 2015



ROCKVILLE OFFICE
 2 Research Place, Suite 100 Rockville, MD 20850 | 301-948-2750 | 301-948-9087
 Engineering | Planning | Surveying | Environmental Sciences
 www.LSlandmark.com



SURVEYOR'S CERTIFICATION

I hereby certify to the best of my professional knowledge, information and belief that the information shown hereon is correct and that it is an ownership plat of Lot 14, Block 5, as shown on subdivision record plat titled LOT 14, BLOCK 5, HALPINE and recorded among the Land Records of Montgomery County, Maryland as Plat 24213; and that it is all of the property described in a conveyance from SMS CORP., a New Jersey corporation, to 1900 CHAPMAN PROJECT OWNER, LLC, a Delaware limited liability company, by Special Warranty Deed dated May 13, 2011, and recorded among the Land Records in Liber 41611 # F-06 425.

And that, once engaged as described in the owner's dedication hereon, all iron pipes marked thus: ———— and monuments marked thus: ———— shall be set as indicated hereon. The total area of this ownership plat is 230,583 square feet or 5.29346 acres of land.

Brian L. Wood
 Professional Land Surveyor
 Maryland Registration No. 10985
 License Expires: 5/6/2016
 Date _____

THE CITY OF ROCKVILLE PLANNING COMMISSION
 ROCKVILLE, MARYLAND

APPROVED - _____
 CITY MANAGER

WITHOUT COMMITMENT TO THE INSTALLATION OF WATER, SEWER AND STREETS
 FOR PUBLIC SEWER AND WATER SYSTEMS ONLY

SRC CONTRACT NO. 04611-315

TWINBROOK PARKWAY
 FORMERLY ROLLINS AVENUE
 (ROW WIDTH VARIES)

RECORDED
 PLAT



City of
Rockville
Get Into It

111 Maryland Avenue | Rockville, Maryland 20850-2364 | 240-314-5000
www.rockvillemd.gov

August 20, 2012

Matt Hurson and Rob Steward
1900 Chapman Project Owner, LLC
555 13th Street, N. W., Suite 102
Washington, DC 20004

Re: Site Plan Application STP2012-00112, 1900 Chapman Avenue

Dear Mr. Sirs:

At its meeting on August 8, 2012, the City of Rockville Planning Commission voted to approve the above-referenced Site Plan Application. The approval allows for the redevelopment of the site with two, six-story buildings containing a total of 660,373 square feet of gross floor area. Building I/Phase I is approved for a total of 319,791 square feet of gross floor area. The approval allows for 319 multiple-unit residential apartments, 3,731 square feet of amenity space, a 1,443 square foot leasing office and 5,152 square feet of office use space in Building I. Building II/Phase II is approved for a total of 335,430 square feet of gross floor area and is approved to contain 339 multiple-unit residential apartments. Site Plan approval is subject to the following conditions:

1. Submission, for approval of the Chief of Planning, of twelve (12) copies of the site plan (on sheets no larger than 24 inches by 36 inches).
2. Submission, for the approval of the Chief of Planning, of twelve (12) copies of the architectural plans (on sheets no larger than 24 inches by 36 inches).
3. Submission, for the approval of the Chief of Planning, of twelve (12) copies of a coordinated detailed landscaping plan (on sheets no larger than 24 inches by 36 inches).
4. Submission, for the approval of the Chief of Planning, of a construction phasing plan to determine site functionality during Phase II of the project. The plan must be submitted and approved prior to the release of the Building Permit for Phase I/Building I.

Forestry

5. In accordance with the requirements of the Forest and Tree Preservation Ordinance, the applicant shall submit for review and approval a Final Forest Conservation Plan (FCP) and obtain a Forestry Permit (FTP) prior to release of the Building permit.
 - a) Comply with the Preliminary FCP approval letter dated June 20, 2012.
 - b) The Final FCP shall be consistent with the Preliminary FCP and Landscape Plan.
 - c) The Final FCP will be reviewed concurrently with the detailed engineering drawings that are submitted to the Department of Public Works.
 - i) Street tree types and locations along the Chapman and Thompson Avenue rights-of-way will be determined at detailed engineering review and permitted under the Public Works permit (PWK) issued by the Department of Public Works.
6. The applicant must execute and record among the Land Records an amended Forest Conservation Easement and Declaration of Covenants in a form suitable to the City prior to issuance of the Forestry permit.
7. The applicant must execute a Five-year Warranty and Maintenance Agreement in a form suitable to the City prior to issuance of the Forestry permit.
8. The applicant must post a bond or letter of credit approved by the City prior to issuance of the Forestry permit.

Department of Public Works

9. Submission, for review, approval, and permit issuance by the Department of Public Works (DPW), of the following detailed engineering plans, studies and computations, appropriate checklists, plan review and permit applications and associated fees:
 - a) Stormwater Management (SWM) Plans;
 - b) Sediment Control Plans for all disturbed areas;
 - c) Public Improvement Plans.
 - d) All detailed engineering plans must be submitted on 24" x 36" sheets at 1" = 30' scale.
10. Submission, for review and approval by the City Attorney's office prior to DPW permit issuance, all necessary deeds, easements, dedications, and declarations.
11. Comply with conditions of the Development SWM Concept Approval Letter dated June 12, 2012.
12. Comply with conditions of the Water and Sewer Authorization Letter dated June 18, 2012.
13. Post financial securities based on the approved construction estimate in a format acceptable to the City Attorney. Approval is coordinated through DPW staff.
14. Obtain all required permits from any agencies or jurisdictions, including WMATA and Montgomery County, for any work within their easements or public rights-of-way.

Traffic/Pedestrian Circulation and Traffic Mitigation

15. The applicant shall provide hardscape along Thompson and Chapman Avenues when adjacent to parallel parking. When there is no parallel parking on these streets, the applicant will be permitted to provide a tree lawn. The design of the hardscape shall be approved by Department of Public Works prior to issuance of the public works permit.
16. All internal traffic control devices (i.e. signs, marking and devices placed on, over or adjacent to a roadway or walkway) to regulate, warn or guide pedestrians and/or vehicular traffic shall comply with the latest edition of the Manual on Uniform Traffic Control Devices (MUTCD). The signing and pavement marking plans shall be submitted to DPW and approved by the

Chief, Traffic and Transportation Division. (This plan shall be approved and included in the signature set.)

17. The applicant shall provide long-term (bicycle lockers) storage and short-term (bicycle racks) storage for two buildings as follows:
 - i) Building 1: 55 bicycle lockers (110 long-term spaces) and 4 bicycle racks (8 short-term spaces); and
 - ii) Building 2: 57 bicycle lockers (114 long-term spaces) and 4 bicycle racks (8 short-term spaces).

A locked and covered bicycle room with racks or a bicycle locker is required for long-term storage. Short-term spaces are considered to be an inverted "U" bicycle rack and must be spaced four feet apart. These spaces shall be provided at a safe and secure location approved by DPW during the detailed engineering stage. Bike lockers and racks must be installed prior to issuance of the occupancy permit.

18. The applicant shall submit for review and approval by DPW, a phasing plan for pedestrian access, construction access, staging and parking. A pedestrian access plan for the construction period shall include, but not be limited to, the methods of maintaining pedestrian safety and access on the existing sidewalks, temporary closing of sidewalks for work in the streetscape zone, and pedestrian detours, as well as efforts to minimize closure of sidewalks. This plan shall be approved prior to issuance of the building permit.
19. The Applicant must pay the City's Transportation Improvement Fee as provided in the Comprehensive Transportation Review (CTR). The fee is \$1.50 per square foot of non-residential gross floor area (\$7,728) and \$900 per multi-family unit (\$600,300). The total of \$608,028 must be paid prior to the issuance of the building permit.
20. The applicant shall install Washington Globe streetlight poles (M1925-37-14, Manufactured by Union Metal) with style "g" luminaire (100-watt HPS). The finish color of all external components shall be Federal Standard Color #14036, per Federal Standard 595a (1984). Final streetlight specifications, locations and spacing shall be approved by DPW during detailed engineering stage.
21. Permanent tie-backs are not permitted in the public right-of-way.
22. Locations of parking gates and/or booths at all garage entrances shall be approved by DPW prior to issuance of the building permit.
23. Submission, for review and approval by DPW, of the upgraded sidewalk section and the sidewalk finishes located within the public right-of-way and public sidewalk access easements on Chapman and Thompson Avenues sidewalks. The design shall be approved by DPW during the detailed engineering stage and will be permitted with the Public Improvement Plans.
24. The applicant shall prepare the following easements and maintenance agreements in a form acceptable to the City Attorney's office and record in the Montgomery County land records prior to the issuance of the building permit.
 - i) Thompson Avenue – A six foot wide public access easement (PAE) measured from the right-of-way line. A maintenance agreement is required for the entire upgraded sidewalk including the hardscape located adjacent to the parallel parking (see Condition #1)
 - ii) Chapman Avenue – A 7-foot PAE measured from the right-of-way line. A maintenance agreement is required for the entire upgraded sidewalk including the hardscape located between the street parking and sidewalk adjacent to Building 1.
 - iii) Service Drive (parallel to Thompson Avenue portion) – A 54 foot public access easement which includes the sidewalk, travel lanes and parking lanes.

- iv) Service Drive (parallel to Chapman Avenue portion) – A 32 foot wide public access easement which includes the sidewalk and travel lanes.
- 25. Traffic Mitigation - The applicant shall design, construct and obtain permits from Montgomery County, State Highway Administration (SHA), and/or the City of Rockville as required to mitigate at the Twinbrook Parkway/Rollins Avenue and Rockville Pike intersection. The applicant will construct a westbound right turn lane along Twinbrook Parkway to northbound Rockville Pike. The applicant will reconstruct a five foot wide sidewalk. Plans for the mitigation must be approved prior to the issuance of the building permit for the second building. Mitigation is required to be completed prior to issuance of the occupancy permit for the second building.
- 26. The applicant shall pay the County's Development Impact Tax, as applicable, subject to allowable credits allowed by Montgomery County, prior to issuance of the building permit. The applicant shall submit a receipt of payment to Inspection Services and Traffic and Transportation Division prior to issuance of the building permit.

Publicly Accessible Art in Private Development

- 27. The Applicant shall comply with the City's Publicly Accessible Art in Private Development Ordinance. The applicant must provide a concept plan for approval prior to issuance of a building permit and meet the art requirement prior to issuance of an occupancy permit for the building. The manual for this ordinance can be found on the City's website www.rockvillemd.gov, click on business.

General

- 28. That the applicant shall comply with Montgomery County's Solid Waste and Recycling requirements.
- 29. Permits for all signs must also be obtained from the Planning Division.
- 30. All new transformers be placed underground, unless a waiver from the Planning Commission is granted.
- 31. That truck pick-up and delivery on-site is prohibited between 9:00 p.m. and 7:00 a.m. Monday through Friday and between 9:00 p.m. and 9:00 a.m. on Saturday, Sundays and federal holidays.
- 32. That the applicant meet all applicable handicap-accessibility requirements of the State of Maryland and the Americans with Disabilities Act of the Federal Government, as well as all construction code requirements of the City of Rockville.
- 33. That Moderately Priced Dwelling Units (MPDUs), be provided in accordance with the requirements of the Moderately Priced Housing Ordinance.
- 34. The applicant shall sign and return the approval letter prior to the release of any permits.

The Planning Commission adopted the following findings in accordance with Section 25.07.01a.3.a which states that a site plan application that does not implement a project plan or a Special Exception may be approved only if the applicable Approving Authority finds that the application will not:

- a) *Adversely affect the health or safety of persons residing or working in the neighborhood of the proposed project;*
The development of this site with residential apartments will bring life to this site nearly 24 hours a day. The retail development closed down nearly 12 hours, leaving it vacant adjacent to Chapman Avenue, which is an important pedestrian route to the Metro station. Having residential apartments along Chapman Avenue should result in an added sense of security

for pedestrians using this street. Therefore, instead of adversely affecting the health or safety of persons residing or working in the neighborhood of the proposed project, their health and safety should be improved.

- b) *Be detrimental to the public welfare or injurious to property or improvements in the neighborhood;*
The redevelopment of this property changes it from a retail building with a vast parking lot to a development that is built to the scale envisioned by the Master Plan. Its location provides residents with the ability to live within 500 feet of the Metro station where they can take advantage of available public transportation. The density with 658 multi-unit residential apartments is significant but the impacts associated with traffic have been evaluated and compensatory mitigation is being conditioned upon approval. Better sidewalks are proposed around the site for pedestrian activity. The trees being planted on the site are a vast improvement over the existing perimeter and parking lot island trees currently located on site. Since no detrimental aspects have been identified with respect to the proposed development and since it affords improvements to the site and the area around it, while complying with the requirements of the Zoning Ordinance and the Master Plan, the development, as proposed, will not be detrimental to the public welfare or injurious to property or improvement in the neighborhood.
- c) *Overburden existing and programmed public facilities as set forth in Article 20 of this Chapter and as provided in the adopted Adequate Public Facilities Standards;*
As noted above, the school capacity is under the set limit of 110% of capacity, including the reserved capacity for the two surrounding City projects that have recently been approved. Further, adding in the approved projects does not cause the capacity in the Farmland Elementary, Tilden Middle, and Walter Johnson High Schools to exceed the 110% capacity limitation. No significant impacts on emergency services are anticipated as a result of this project. Station 23 is located on Rollins Avenue, very close to this site. Further, none of the uses proposed with this mixed-use project are considered high risk and do not require full response from three stations within 10 minutes. Full compliance with the Department of Public Works requirements will ensure that there is no impact on public sewer and water facilities as a result of this development. The sewer and water improvements necessary to accommodate this project as well as the projects at 1592 and 1800 Rockville Pike will actually improve the public sewer and water facilities in the area. Mitigation for all traffic impact has been identified and will need to be permitted and completed prior to occupancy of any portion of either building. Based on the above, the proposed development will not overburden existing and programmed public facilities as set forth in Article 20 of this Chapter and as provided in the adopted Adequate Public Facilities Standards;
- d) *Adversely affect the natural resources or environment of the City or surrounding areas;*
The site is currently developed with a single retail building with a surface parking lot. There is minimal existing landscaping and trees are limited to the area around the perimeter of the property and within parking islands. There are no streams, habitat, or natural resources located on the property. No other environmental concerns are known that would affect the natural resources or environment in the surrounding area. The proposal will also add significant tree cover to the site. As a result, the proposed development will not adversely affect the natural resources or the environment of the City or surrounding areas.

e) *Be in conflict with the Plan;*

The proposed development is not in conflict with the Plan. It implements development in keeping with the proposals suggested for the area around the Twinbrook Metro Station. It is believed that the proposal improves pedestrian circulation and employs urban design principles to create an attractive streetscape. It contains a development with a strong residential component within the Metro Performance District, improves the physical appearance of the property, is ideally sited for public transit usage, improves pedestrian and bicycle circulation and the conditions as approved implement improvements to ease traffic congestion. The parking complies with the requirements of the Zoning Ordinance, and the proposed development will not be in conflict with the Plan because the Zoning Ordinance, in essence, implements the Master Plan. As a result, the proposed application will not be in conflict with the Plan.

f) *Constitute a violation of any provision of this this Chapter (the Zoning Ordinance) or other applicable law;*
or

The project has been carefully reviewed and it meets the requirements of the Zoning Ordinance. As noted in the various sections above, the forestry, transportation and sewer and water requirements are being met. Since all aspects have been reviewed and addressed either through the site layout and design, mitigation, or conditions, the proposed development will not constitute a violation of any provision of the Zoning Ordinance or other applicable law.

g) *Be incompatible with the surrounding uses or properties.*

The proposed use of the property is in compliance with the permitted uses contained in the Zoning Ordinance. Further, it proposes development in keeping with the 1989 Rockville Pike Corridor Plan and the Master Plan by proposing multi-unit dwellings in an area designated for higher density development. The property is also located along a pedestrian route for occupants of the surrounding neighborhoods between their homes and the Metro Station. Developing this property with a use that will provide occupancy 24 hours a day will serve to enhance the surrounding communities. As a result, the proposed development will not be incompatible with the surrounding uses or properties.

In a separate motion, the Planning Commission granted a waiver from the requirement that the electrical transformers be undergrounded or located within an enclosed building based on Section 25.17.03.c.1(a) and (b). The Commission's found that the site contained a unique or peculiar site condition that provides a physical impediment to installing equipment underground in that it would be unsafe to locate the equipment underground based on the underground contamination that exists on the site.

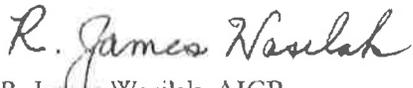
A deviation from the environmental guidelines was also granted by the Planning Commission in approving the application. The noise study submitted with the application determined that the noise levels exceeded acceptable levels in the outdoor area between Building II and the railroad tracks due to train noise and general traffic noise. The Commission found that a sound wall which would mitigate the noise, would adversely impact the development and the area in a way that did not correspond to the degree of the noise levels and frequency due to its height, its dimension and its aesthetics.

Matt Hurson and Rob Steward
Page 7
August 20, 2012

Section 25.07.06 of the Zoning Ordinance requires that construction must commence pursuant to a validly issued Building Permit within two (2) years of the effective date of the Planning Commission approval, which is the date of this letter. However, the Planning Commission may, for good cause shown, grant no more than two (2) extensions of not more than six (6) months each for any approval subject to the provisions of Section 25.05.08 "Extension of Implementation Period."

Pursuant to Section 25.04.02.f, any person aggrieved by a final decision of the Planning Commission may appeal same to the Circuit Court for Montgomery County, taken according to the Maryland Rules set forth in Title 7, Chapter 200.

Sincerely,



R. James Wasilak, AICP
Chief of Planning

cc: Planning Commission
Nancy Regelin, Shulman Rogers
Jon Wallenmeyer, SK+I Architectural Design Group
K. C. Reed, Loiederman Soltez and Associates
Carl Starkey, Street Traffic Studies
Marcy Waxman, Senior Assistant City Attorney
Bobby Ray, Principal Planner
Mark Wessell, Engineering Supervisor
Elise Cary, Assistant City Forester
Peter Campanides, Civil Engineer III
Rebecca Torma, Transportation Planner III
Dan Kane, Civil Engineer I
Joe McClane, President, Cambridge Walk II Homeowners Association
Dan Ryan
George and Suzi Tatarian
Frederico Olivera-Sala
Muge Karakus
Sam Shipkovitz
Brigitta Mullican
Judy Miller, Vice-President, Twinbrook Citizens Association
Christina Ginsberg, President, Twinbook Citizens Association
Anne Goodman
Jim Farrelly
Erin Fitzgerald
Cheryl Cort, Coalition for Smarter Growth
File
Reading File



THIS PLAN IS THE PROPERTY OF DEWBERRY AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF DEWBERRY.

SCALE



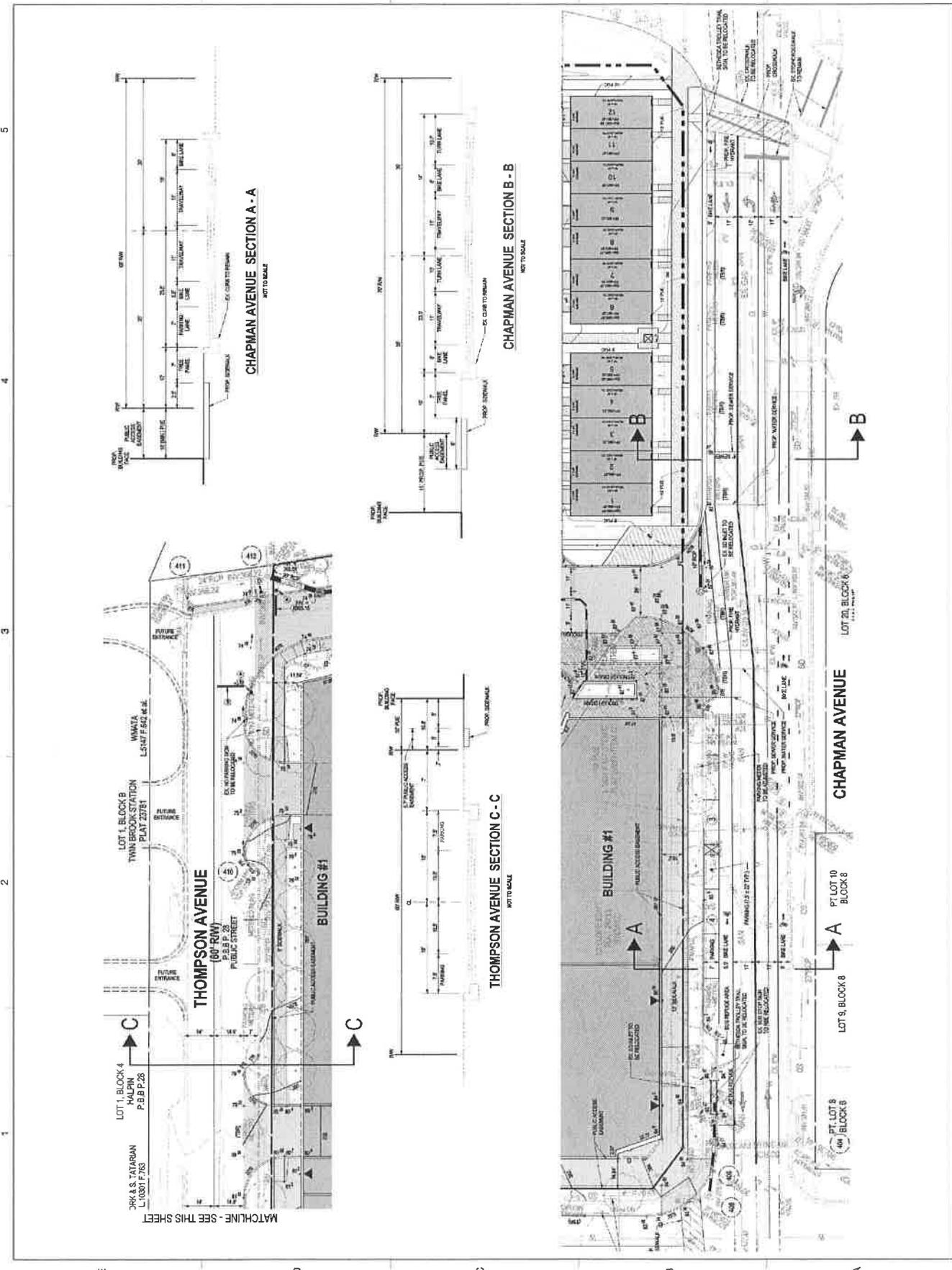
REVISIONS	DATE
DESIGNED BY	DATE
DRAWN BY	DATE
APPROVED BY	DATE
CHECKED BY	DATE
DATE	MARCH 2015
TITLE	

Public Street Frontage
 Improvement Plan

PROJECT NO. 3008801

C-8

SHEET NO. 8 OF 28



7101 Calver Road, Suite 340
 Rockville, MD 20850-4037
 Tel: 301.253.1000
 www.dewberry.com

PROJECT
 1900 Chapman Project Owner, LLC
 1800 15th Street NW, Suite 600
 Washington, DC 20001
 Attn: Matthew Huron
 Telephone: (202) 634-0353
 Email: matthew.huron@dewberry.com

1900 Chapman Avenue
 City of Rockville, Maryland
 Level 2 Site Plan
 Major Site Plan Amendment to
 STP2012-0112
 TAX MAP GD563
 TAX ACCT. 803187110
 ZONE - MKTD



SCALE
 1" = 10'-0"

REVISIONS

NO.	DATE	BY	DESCRIPTION
1	01/15/12	PH	ISSUED FOR PERMIT

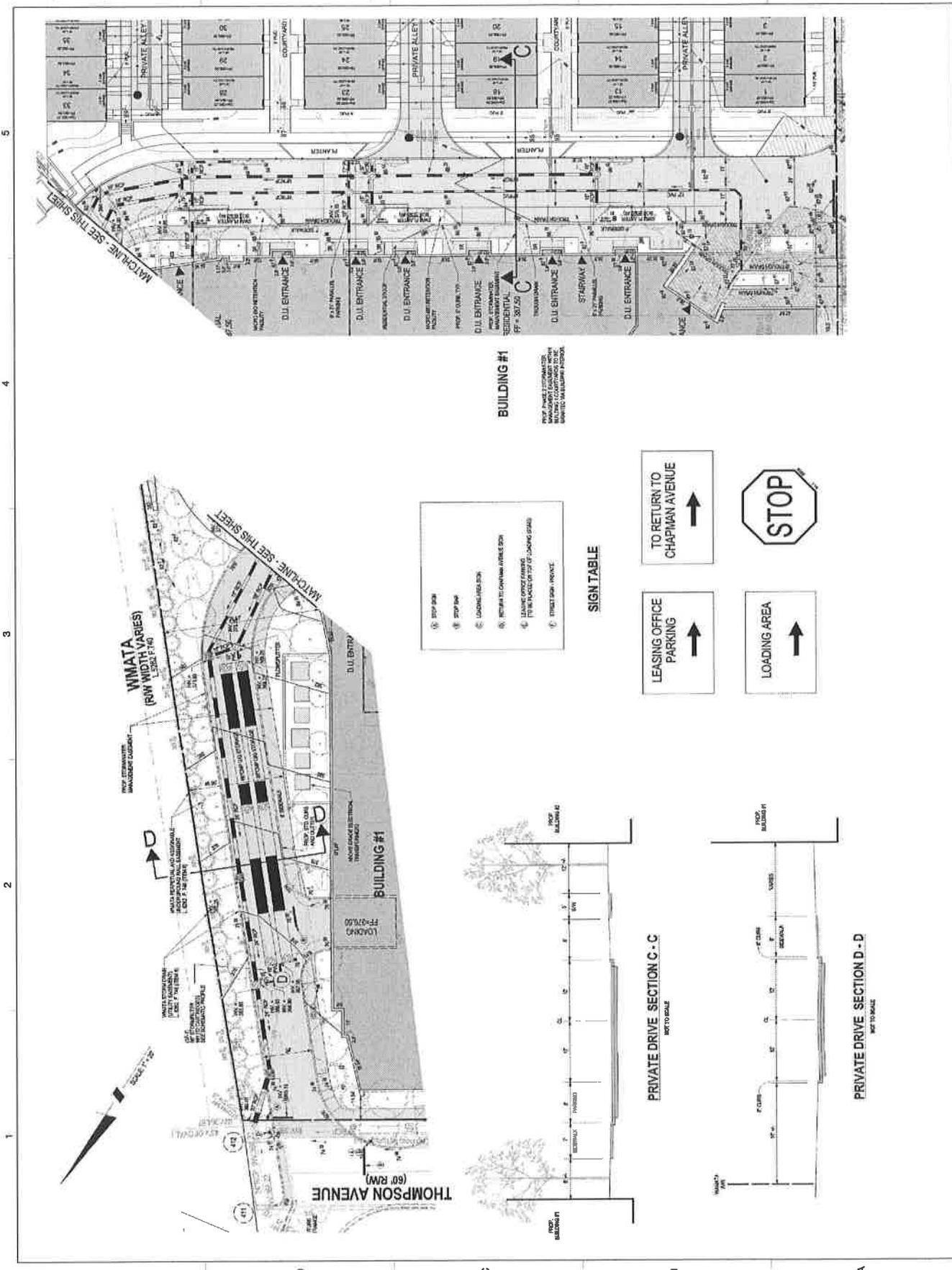
DESIGNED BY PH
CHECKED BY WJ
DATE MARCH 2012

Private On-Site Drive Improvement Plan

PROJECT NO. 8080801

C-9

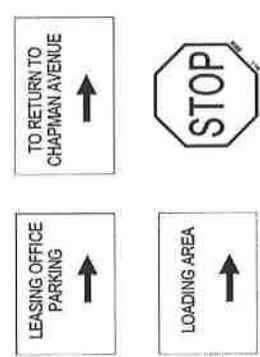
SHEET NO. 9 OF 29



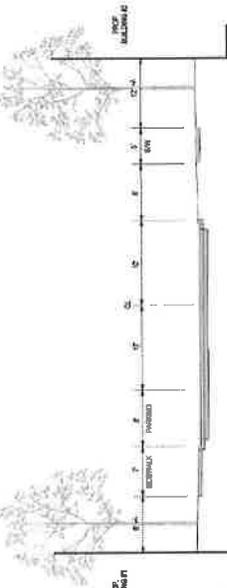
BUILDING #1
 PROP. FINISHES TO BE DETERMINED BY THE ARCHITECT. ALL FINISHES TO BE MATCHED TO BUILDING INTERIOR.

- 1. STOP SIGN
- 2. STOP SIGN
- 3. LOCKSMITHED SIGN
- 4. SIGNAGE TO CHAPMAN AVENUE SIDE
- 5. LEASING OFFICE SIGNAGE (TO BE PLACED ON TOP OF LOADING GRAB)
- 6. TRAFFIC SIGN - PAVEMENT

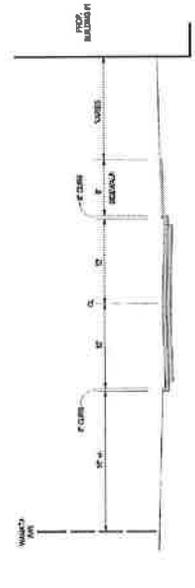
SIGN TABLE



PRIVATE DRIVE SECTION C-C
 NOT TO SCALE



PRIVATE DRIVE SECTION D-D
 NOT TO SCALE



2501 Beltsville Road, Suite 340
 Rockville, MD 20850-3437
 301.946.3000 Fax: 301.258.7667
 www.dewberry.com

CLIENT
 1900 Chapman Project Owner, LLC
 20 Parkview
 Washington, DC 20001

ARCHITECT
 Alex Mathew-Hansen
 Telephone: (202) 614-6253
 Email: mathew.hansen@pha.com

1900 Chapman Avenue
 City of Rockville, Maryland
 Major Site Plan Amendment to
 STP2012-00112
 TAX ACCT: 80318710
 TAX MAP CODES
 ZONE - AKTD

SCALE
 NOT TO SCALE

NO.	DATE	BY	DESCRIPTION

REVISIONS

DESIGNED BY: []
 DRAWN BY: []
 CHECKED BY: []
 DATE: []
 TITLE: []

Phase 2 - Townhouse Architecture

PROJECT NO.: 1900PH21

SHEET NO.: A4.01

COURSE WINDOW NUMBER

*Typical Gable and High-Visibility Gable Dimensions to be modified for 2 Story Foot Load Townhouses



GENERAL NOTES

1. The information on this plan was prepared by the City of Rockville Planning Department.
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19. The information on this plan was prepared by the City of Rockville Planning Department.
20. The information on this plan was prepared by the City of Rockville Planning Department.

LEGEND

- OPEN SPACE
- EXISTING TREE
- NEW TREE (10' DIAMETER)
- NEW TREE (6" DIAMETER)
- NEW TREE (4" DIAMETER)
- NEW TREE (2" DIAMETER)
- NEW TREE (1" DIAMETER)
- NEW TREE (0.5" DIAMETER)
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2101 Calvert Road, Suite 400
 Rockville, MD 20850-4207
 301.540.8300 Fax: 301.562.7607
 www.dewberry.com

PROJECT
 1900 Chapman Project Owner, LLC
 60 New Street, Suite 400
 Washington, DC 20001

ARCHITECT
 Major Site Plan Amendment to
 STRP2012-00112
 City of Rockville, Maryland
 1900 Chapman Avenue
 Rockville, MD 20850

1900 Chapman Avenue
 City of Rockville, Maryland
 Major Site Plan Amendment to
 STRP2012-00112
 TYPICAL ACCESS LANE
 ZONE - MIXED



SCALE
 1" = 10'-0"

NO.	DATE	BY	DESCRIPTION

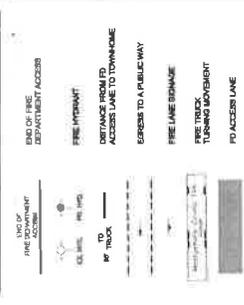
DATE: 11/20/12
 DRAWN BY: JACOTT
 APPROVED BY: [Signature]
 CHECKED BY: [Signature]
 DATE: 11/20/12
 TITLE: Fire Protection Site Plan - Phase 2

PROJECT NO.: 1900001
 SHEET NO.: 1 OF 1



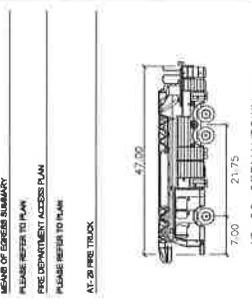
VICINITY MAP
 SCALE: 1"=200'

- FIRE PROTECTION SITE PLAN - GENERAL NOTES**
1. Project Name: To be determined by owner. This is a Major Site Plan Amendment to STRP2012-00112. Drawings included as part of the Major Site Plan Amendment include but are not limited to the modification of the following:
 2. Property Address: 1900 Chapman Avenue, Rockville, Maryland 20850
 3. Total Area: 0.25 AC
 4. Total Area: 0.25 AC
 5. Total Area: 0.25 AC
 6. Total Area: 0.25 AC
 7. Existing Conditions: Former 5700 sq. ft. Commercial use with existing safety hazard. The site is not subject to City of Rockville 1800 review.
 8. Historic District: The site is not subject to City of Rockville 1800 review.
 9. Existing topography and boundary survey prepared by [Signature], 2011.



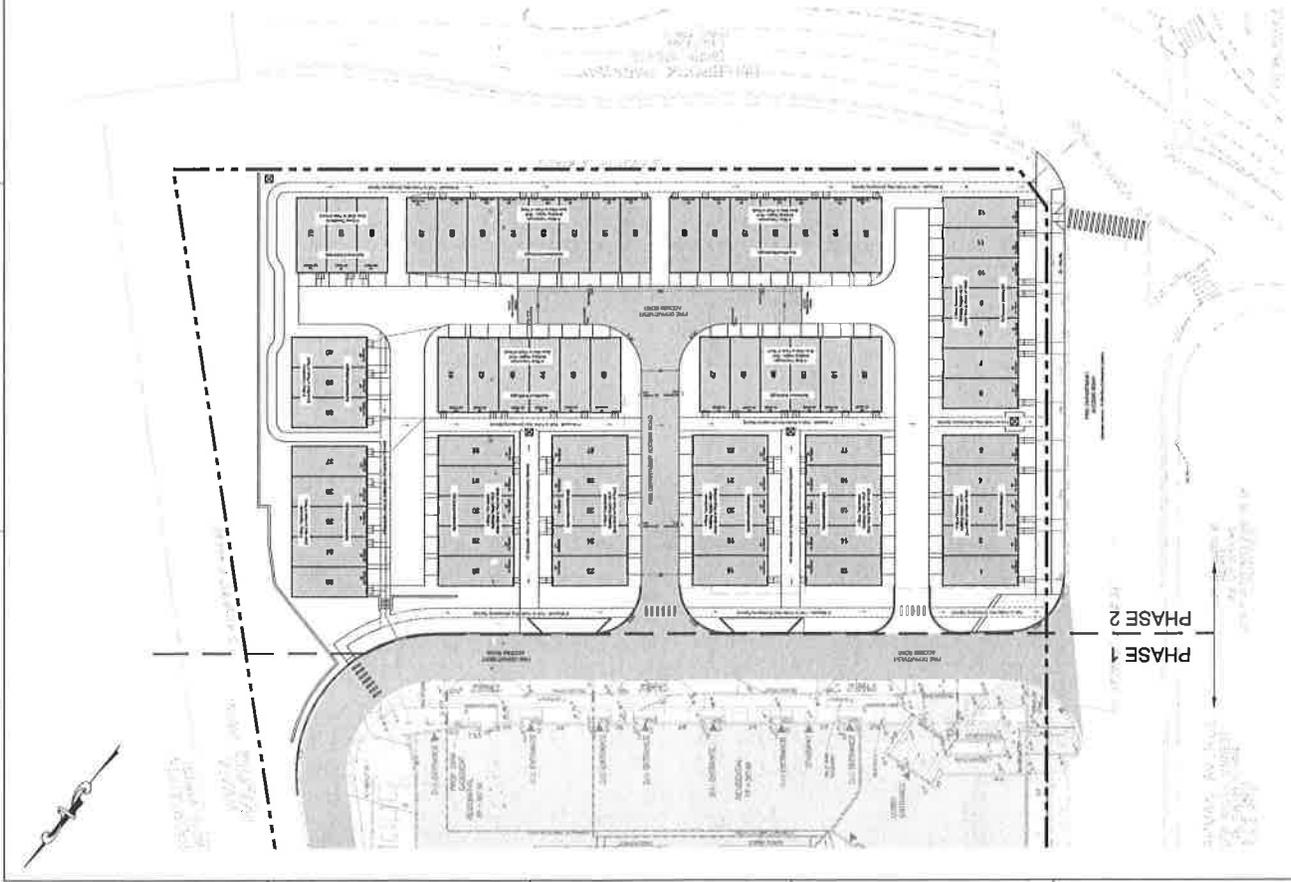
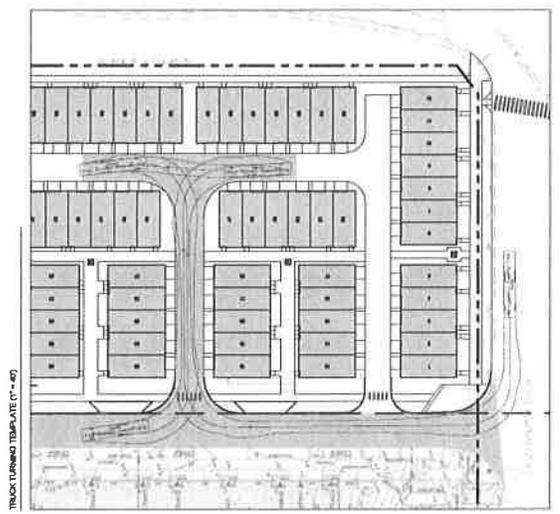
BUILDING CODE SUMMARY - PHASE 2

CONSTRUCTION TYPE: Type III
 FIRE PROTECTION SYSTEMS: Automatic Building
 AREA CALCULATIONS: 10,700 sq. ft. (10,700 sq. ft.)
 FIREHAZARD: PERMITTED, but necessary for an open structure.



AT-29 GERMANTOWN (ref)
 Height: 8'25"
 Length: 21'75"
 Lock to Lock Time: 6'0"
 Steering Angle: 33.2°

END OF FIRE DEPARTMENT ACCESS - INVERTMENT MARKER
 WIDTH OF FIRE ACCESS LANE (20 FT MIN.)



PHASE 1
 PHASE 2

END OF FIRE DEPARTMENT ACCESS

ATT 7

2101 Calvert Road, Suite 340
 Rockville, MD 20850-4037
 Tel: 301.250.1600
 www.dewberry.com

PROJECT
 City of Rockville
 Chairman Project Owner, LLC
 450 H. Ross Street, NW, Suite 800
 Washington, DC 20001
 Attn: Matthew Hurton
 Telephone: (202) 544-0253
 Email: matthew.hurton@chpwa.com

1900 Chapman Avenue
 City of Rockville, Maryland
 Level 2 Site Plan
 Major Site Plan Amendment to
 STP2012-00112
 TAX MAP G0563
 ZONE - MKTD



DATE: MARCH 2018
 PROJECT NO: 5000001

DESIGNED BY: [Name]
 CHECKED BY: [Name]
 DATE: MARCH 2018
 TITLE: [Title]

REVISIONS
 DRAWN BY: [Name]
 APPROVED BY: [Name]
 CHECKED BY: [Name]
 DATE: MARCH 2018
 TITLE: [Title]

SCALE: 1" = 10'-0"

Open Space Exhibit

PROJECT NO: 5000001
 SHEET NO.: 13 OF 38
 E-2



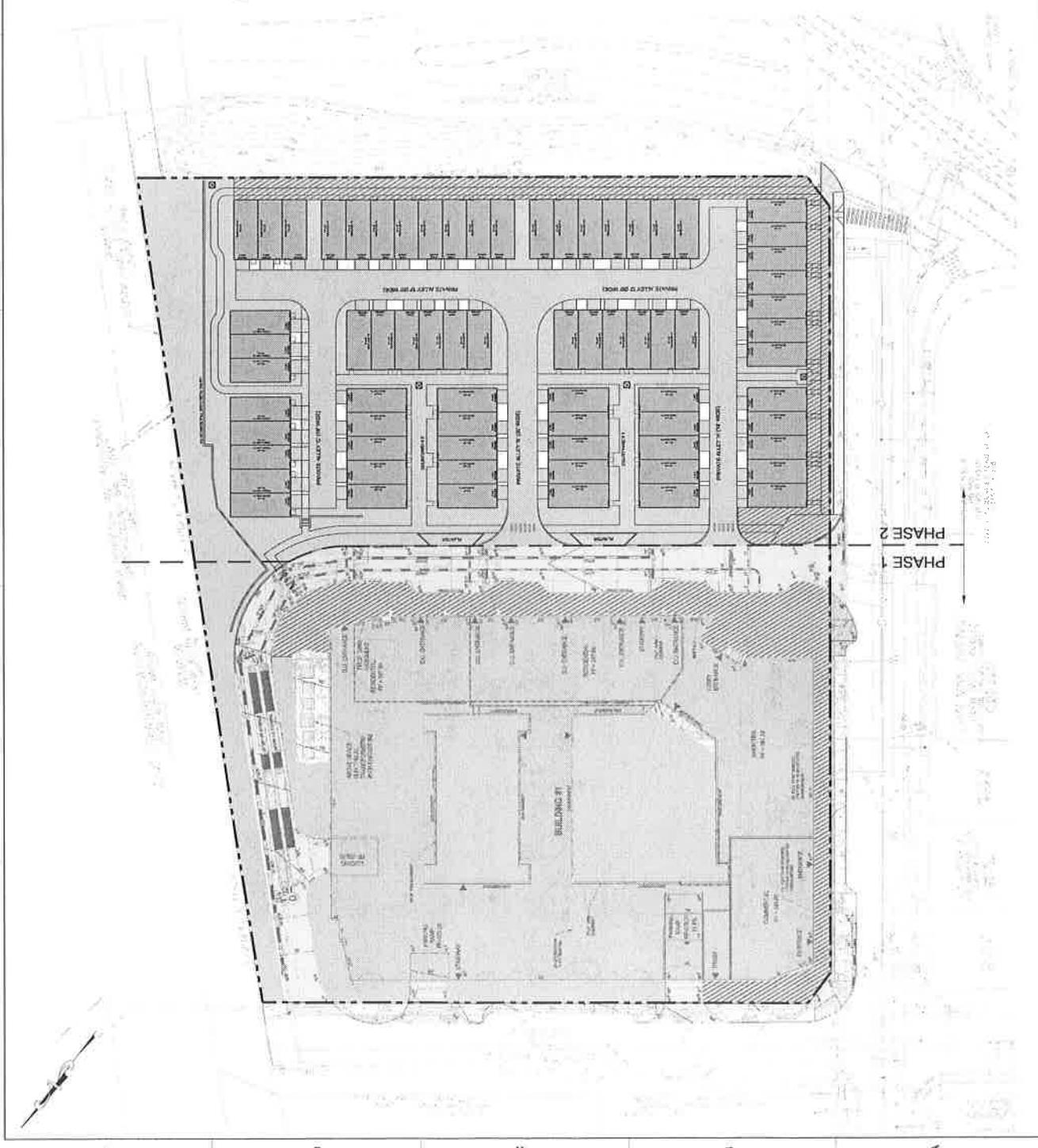
VICINITY MAP
 SCALE: 1/4"=100'

LEGEND

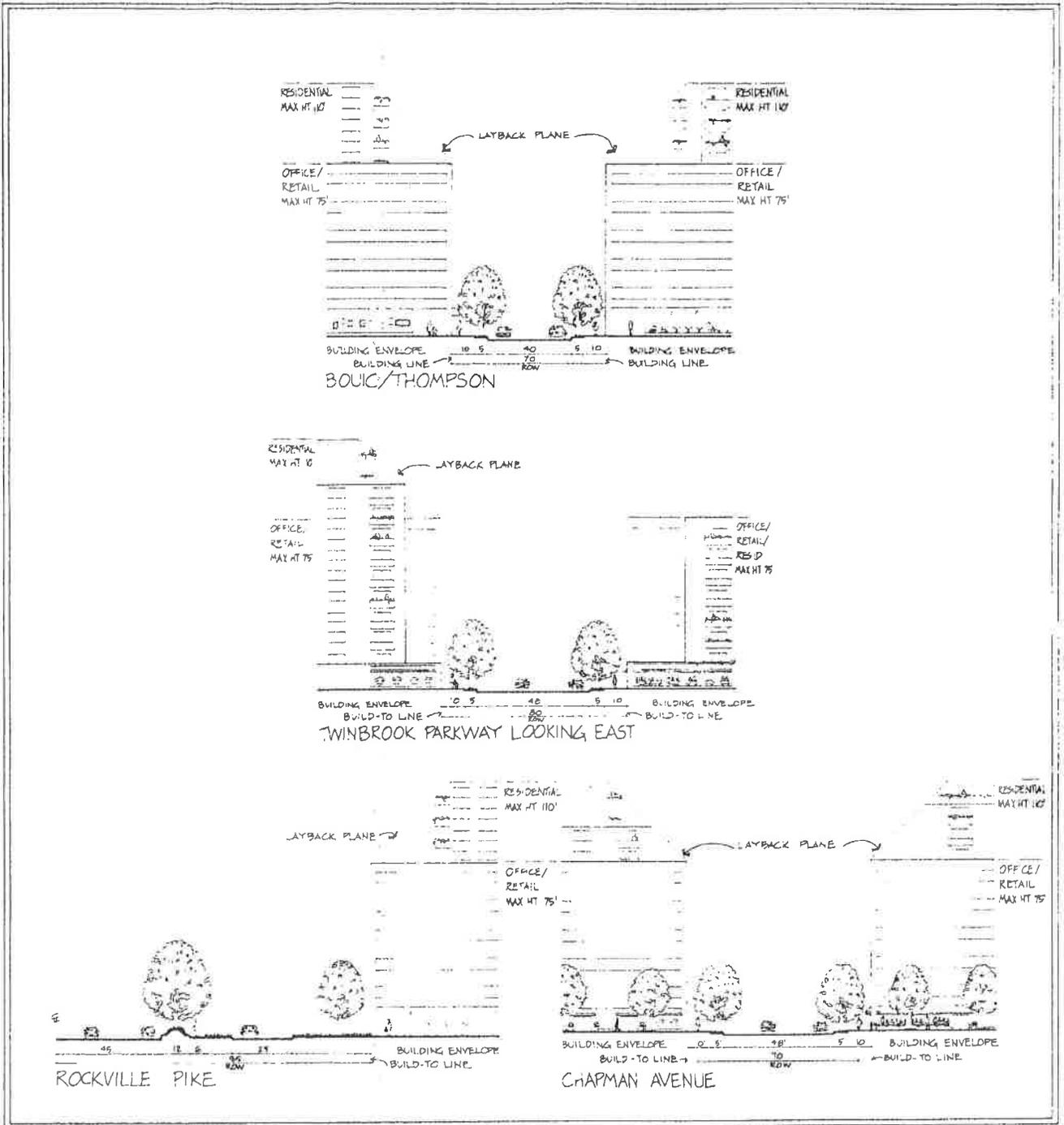
PROPERTY AREA: 1/8" AC (0.0432 SF)
 OPEN SPACE
 REQUIRED: 1/4" MIN SF (10%)
 PROPOSED: 1/8" MIN SF (5%)

PUBLIC USE AREA
 REQUIRED: 2/32" SF (10%)
 PROPOSED: 1/16" SF (5%)
 *Note: Public Use Area is 50.17' SF of the City of Rockville Zoning Ordinance.

- EX TOPOGRAPHY
- EX BOUNDARY
- EX ROAD
- EX CURB
- EX SIDEWALK
- EX DRIVEWAY
- EX DRIVEWAY
- EX WATER VALVE & STOPVALVE MANHOLE
- EX WATER VALVE & FIRE HYDRANT
- EX SEWER & SEWER MANHOLE
- PROP TOPOGRAPHY
- PROP ROAD
- PROP CURB
- PROP SIDEWALK
- PROP DRIVEWAY
- PROP DRIVEWAY
- PROP SIDEWALKING
- PROP STOPVALVE & STOPVALVE MANHOLE
- PROP STOPVALVE & STOPVALVE MANHOLE
- PROP SWM GAGEMENT
- PROP SWM PIPE
- PROP BUILDING
- FUTURE ENTRANCE
- PROP WATER, WATER VALVE & FIRE HYDRANT
- PROP SEWER & SEWER MANHOLE



ATT 8



TWINBROOK METRO AREA

FUNCTIONAL PLAN & SECTIONS: PARCEL I, J, K



ATT 10



Street Traffic Studies, Ltd.

—————
TRANSPORTATION REPORT
1900 CHAPMAN AVENUE
ROCKVILLE, MARYLAND
—————

Prepared For:
1900 Chapman Project Owner, LLC



Date: March 27, 2105
Project Manager: Carl F. Starkey, P.E., PTOE
STS Job No.: 6252

Professional Certification. I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland.
License No. 19555 Expiration Date: 2/26/16

COMPONENT A - INTRODUCTION AND EXISTING CONDITIONS

The purpose of this report is to provide a limited Comprehensive Transportation Review covering Components A, B and E for a project which will generate less than 30 peak hour trips during both the morning and evening weekday peak hours of the adjacent street system. The project area is bound by Twinbrook Parkway on the south, Chapman Avenue on the west, and the CSX tracks on the east. The site was previously home to the 70,000 square foot SYMS retail building. That use is planned to be demolished prior to April 2015. The site was previously approved for 658 dwelling units and 5,152 s.f. of commercial space in August 2012. The projected build-out is 36 to 48 months. Access to the site will be from Chapman Avenue and Thompson Avenue. The general location of the facility is illustrated in Exhibit 1. A schematic of the proposed amended site plan is illustrated in Exhibit 2.

With respect to trip generation, it is anticipated that the project will generate fewer than thirty (30) new peak hour trips as determined using trip generation rates published by MNCPPC Local Area Review and Transportation Area Review Guidelines and the Institute of Transportation Engineers (ITE) *Trip Generation Manual*. Table A was prepared to illustrate the difference in the number of trips between the previously approved uses and the current proposal. As shown, the current proposal produces negative trips during the morning, evening and Saturday peak hours; therefore, a full traffic study is not required pursuant to the City of Rockville Comprehensive Transportation Review (CTR) Guidelines.



STREET TRAFFIC STUDIES, LTD.

*Exhibit 1
Site Location*

TABLE A
TRIP GENERATION - 1900 CHAPMAN AVENUE

	AM PEAK HOUR		PM PEAK HOUR		SATURDAY PEAK HOUR	
	<u>IN</u>	<u>OUT</u>	<u>IN</u>	<u>OUT</u>	<u>IN</u>	<u>OUT</u>
General Office - 5,152 sf	7	1	2	10	1	2
Garden Apartments - 319 units	26	105	100	51	81	69
Townhouse - 70 units	6	28	38	20	26	46
Site Total - Proposed	40	134	140	81	108	198
<i>Existing Approved</i>						
General Office - 5,152 sf	7	1	2	10	1	2
Garden Apartments - 658 units	53	213	205	105	155	132
Site Total - Approved	60	214	207	115	156	289
New Site Trips	-20	-80	-67	-34	-48	-91

(Proposed minus Approved)

Trip Generation rates taken directly from MNCPPC Appendix A and B - Local Area Review and Transportation Area Review Guidelines, supplemented with ITE - Trip Generation Manual for Saturday Volumes, Land Use Code 820, Retail, Land Use Code 710 for Office, and Land Use Code 220 for Apartments and Land Use Code 230 for Residential Condominium/Townhouse

COMPONENT B - SITE ACCESS AND CIRCULATION

Chapman Avenue will provide a direct access to both phases of the project and is a two-lane Business District Street with a north-south orientation. It extends northward from Bou Avenue to its terminus at Halpine Road/Twinbrook Metro Station entrance. It has a posted speed limit of 30 mph.

Thompson Avenue is also a two-lane Business District Street with an east-west orientation. Its limits are Rockville Pike to the west, and the CSX tracks to the east. Thompson Avenue will provide direct access to the proposed garage for 1900 Chapman Avenue. It has a posted speed limit of 25 mph.

1900 Chapman Avenue is proposing a total of 531 (391 for Phase 1, and 140 for Phase 2) parking spaces to serve residents and guests. As required as part of the August 2012 approval the plan will include 110 long term bicycle spaces and 8 short term bicycle spaces for Lot 14A. Trash pick up for the residential building for Phase 1 is proposed via the access drive on Thompson Avenue. For the townhouse section, trash removal will be the typical house collection methods. That is, a waste removal firm will travel through the townhouse area and collect waste from individual trash receptacles. Pick ups are anticipated to occur two to three times weekly.

Circulation through the Townhouse area will be safe and efficient as evidenced via the Fire Access Site Plan (see Exhibits 3 and 3A). Fire apparatus and waste removal trucks will have adequate turning radii within the Townhouse section to operate vehicles without infringement on neighboring homes.

The peak hourly flow rates (two-way) along Thompson Avenue are 95 vehicles per hour (vph) during the morning peak hour and 112 vph during the evening peak hour¹. The traffic flow on Chapman Avenue along the site frontage is 462 vph during the morning peak hour and 841 vph during the evening peak hour¹. The intersection of these two roadways operates at Level of Service 'A' during both peak hours using the critical lane methodology.

¹ Source: STS LTD Turning Movement Count, May, 2011.



SCALE
 GRAPHIC SCALE
 0 100 200 FT
 1" = 100' 0"

DESIGNED BY	CH2M
APPROVED BY	MADEP
PROJECT NO.	HA1D-00112
DATE	08/07/12
TITLE	Level 2 Site Plan

Fire Protection
 Site Plan

PROJECT NO.	2000001
-------------	---------



VICINITY MAP
 SCALE: 1"=2000'

- MINI COLLECTION OF PLAN CONFORMANCE NOTES**
1. The fire protection system shall be designed and installed in accordance with the requirements of the International Fire Code (IFC) and the International Building Code (IBC) as adopted by the City of Rockville.
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 9. The fire protection system shall be designed and installed in accordance with the requirements of the International Fire Code (IFC) and the International Building Code (IBC) as adopted by the City of Rockville.
 10. The fire protection system shall be designed and installed in accordance with the requirements of the International Fire Code (IFC) and the International Building Code (IBC) as adopted by the City of Rockville.

- KEY**
- 1. Fire Protection Discharge
 - 2. Fire Alarm
 - 3. Fire Alarm Control Panel
 - 4. Fire Alarm Control Panel
 - 5. Fire Alarm Control Panel
 - 6. Fire Alarm Control Panel
 - 7. Fire Alarm Control Panel
 - 8. Fire Alarm Control Panel
 - 9. Fire Alarm Control Panel
 - 10. Fire Alarm Control Panel

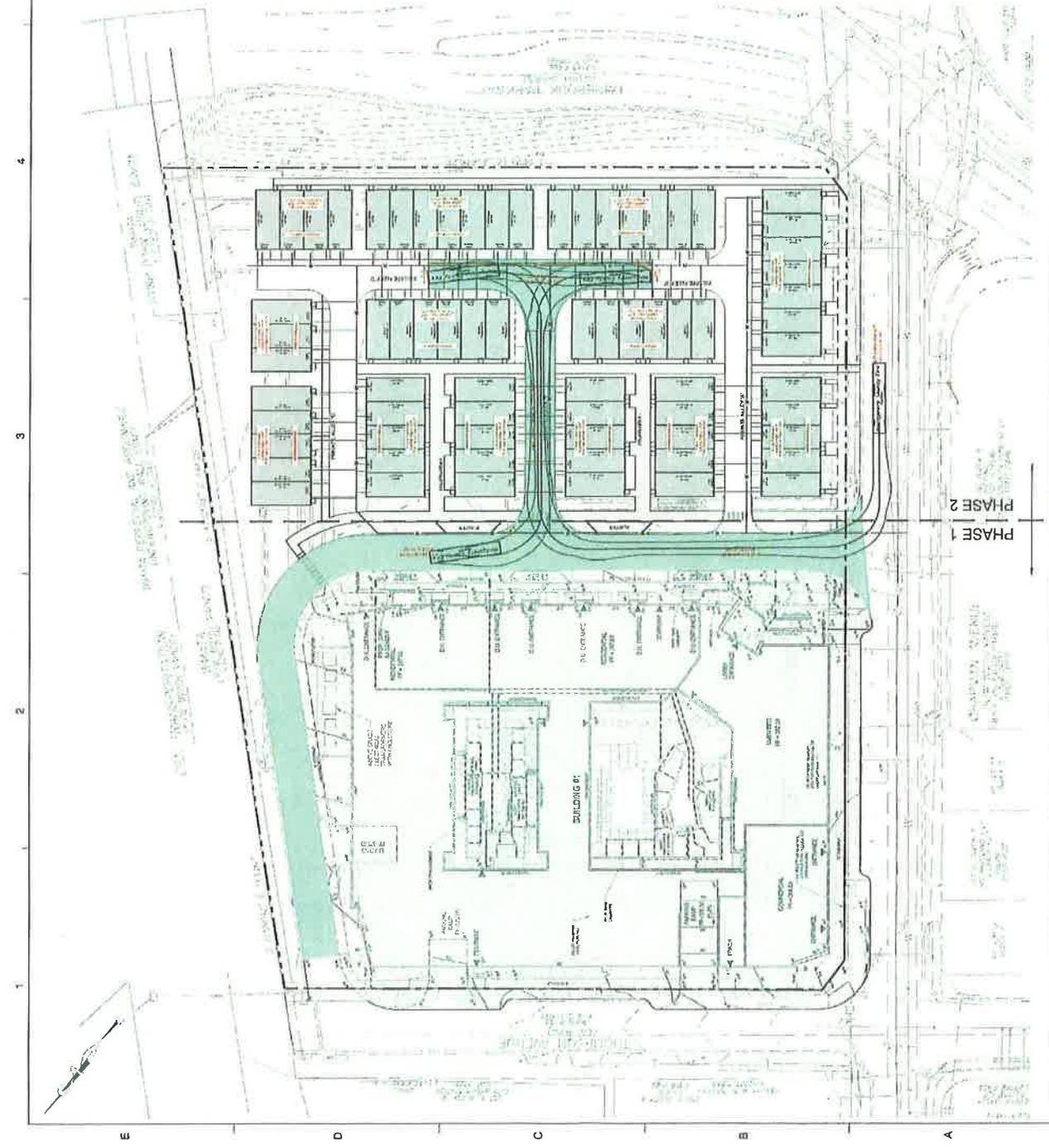
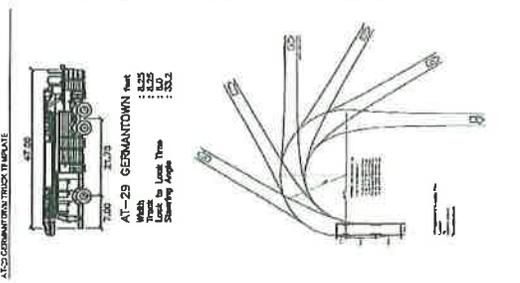


Exhibit 3A

COMPONENT B2- NON-AUTO FACILITIES

Pedestrian Facilities

Within the property boundaries, pedestrian connections will be provided along the internal private drive adjacent to the multifamily building and connecting each townhouse to the private drive sidewalk. The section along the private drive connects the public sidewalk on Chapman and Thompson Avenues. In addition, sidewalk currently exists along the entire western property boundary (Chapman Avenue), and along the southern boundary (Twinbrook Parkway). The existing sidewalk network provides direct pedestrian access to the Twinbrook Metro Station. The applicant also proposes to construct sidewalk along the Thompson Avenue frontage.

Bicycle Facilities

The Bicycle Facilities Master Plan designates Chapman Avenue as an existing Signed-Shared Roadway for cycling. The proposed design will continue to honor this designation. As previously noted, the applicant plans to provide short and long term bicycle parking spaces to encourage their usage.

Transit Facilities

Montgomery County Ride On Bus Route 26 operates along Chapman Avenue, with a stop at the Twinbrook Metro Station. A Bus stop exists in the northwest quadrant of the intersection of Chapman Avenue and Thompson Avenue. The average headway is 30 minutes. It should be noted that the existing Bus Stop at the intersection of Thompson Road and Chapman Avenue is planned to be relocated south closer to the townhouses near Twinbrook Parkway as part of the development process. Information outlining the Route 26 schedule and excerpts from the Guidelines pertaining to Bicycle Routes and Transit Oriented Areas is contained in the Appendix.

COMPONENT E - SUMMARY

The project area is bound by Twinbrook Parkway on the south, Chapman Avenue on the west, and the CSX tracks on the east. The site was previously home to the 70,000 square foot SYMS retail building. That use is proposed to be demolished in April 2015. The site was previously approved for 658 dwelling units and 5,152 s.f. of commercial space in August 2012. The current proposal is for 319 multi-family units, 5,152 s.f. of commercial space and 70 townhouse units. The projected build-out is 36 to 48 months.

This report has been prepared in accordance with the Comprehensive Transportation Review Methodology, but is limited to a discussion of only 3 of the 5 components, A, B, and E, as discussed with City of Rockville staff. Vehicular access to the facility is provided by both Chapman and Thompson Avenues. Sidewalks currently exist along the property frontage on Chapman Avenue.

It is anticipated that the project's current proposal will generate fewer than thirty (30) **new** peak hour trips as determined using trip generation rates published by MNCPPC *Local Area Review and Transportation Area Review Guidelines* and the Institute of Transportation Engineers (ITE) *Trip Generation Manual* when compared to the previously approved proposal. The project is directly accessible via walking from the structural parking for the multifamily building, or the adjacent sidewalks for the townhouses. Since the expected "new" trip totals are less than the 30 trip threshold, a more extensive traffic study is not required.

The report does include a discussion of non-auto facilities in close proximity of the site. In brief, RideOn route 26 operates along Chapman Avenue, with a bus stop in the northwest quadrant of the intersection of Chapman Avenue and Thompson Avenue.

In summary, the proposed Site Plan Amendment will have no impact on the roadway network surrounding the site. Based on the amended site plan for Lot 14B, the prior Condition 25 of the August 20, 2012 approval regarding Traffic mitigation at the intersection of Rockville Pike and Twinbrook Parkway/Rollins Avenue is no longer required due to the significant trip reduction for the 70 townhouse units versus 339 multifamily units. Therefore, this report does not recommend any mitigation measures.



®

APPENDIX

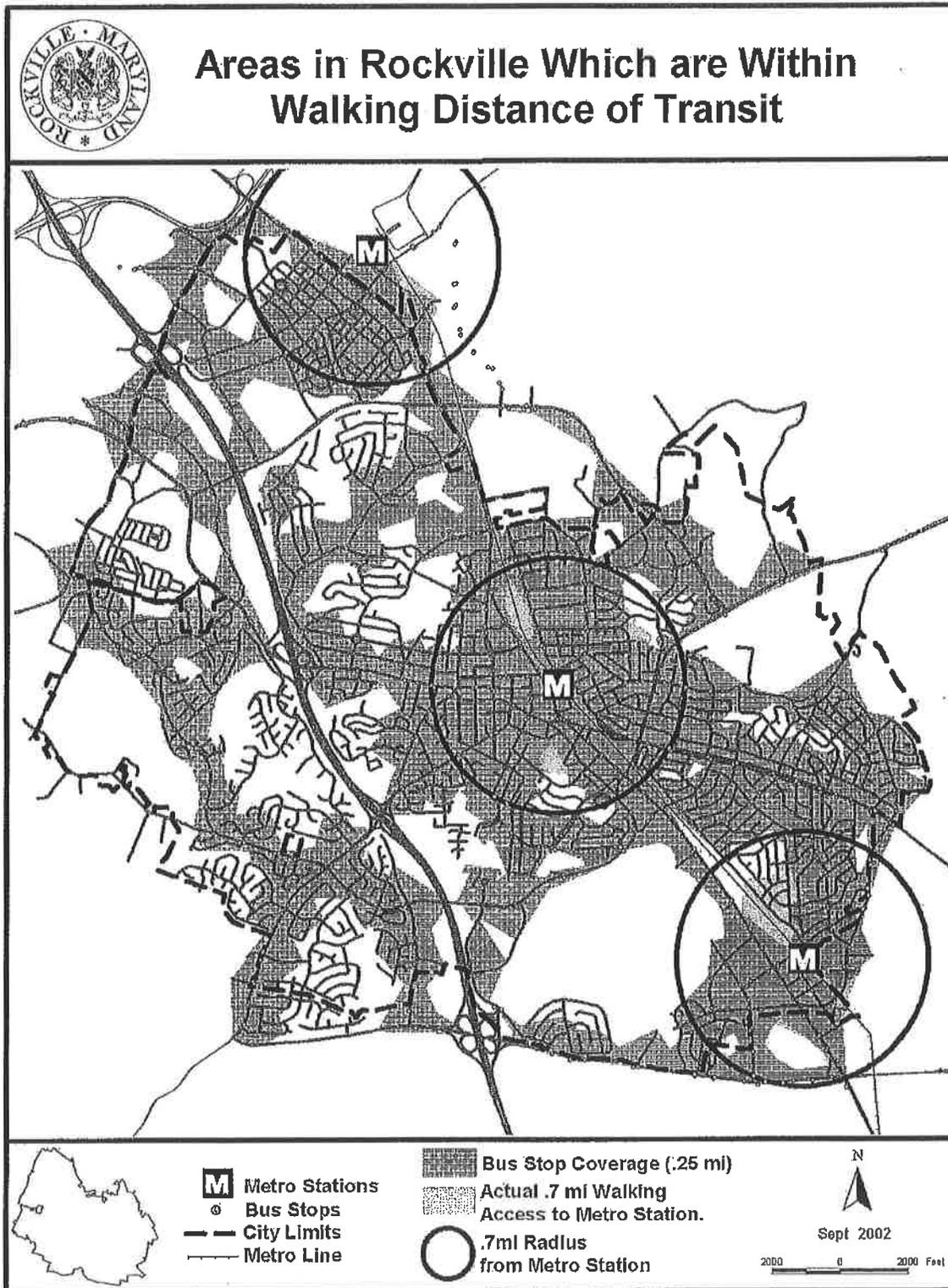


Figure 2. Rockville Bikeway Recommendations

Recommended Bikeways

- Shared-Use Path
- On-Street Bike Lane
- Signed-Shared Roadway*

Existing Bikeways

- Shared-Use Path
- On-Street Bike Lane
- Signed-Shared Roadway*
- Millennium Trail

* Incorporate traffic-calming measures and chicanes on signed-shared roadways, where possible.

- Intersection Improvement
- ★ Pedestrian Bridge
- Ⓜ Metro Rail Station

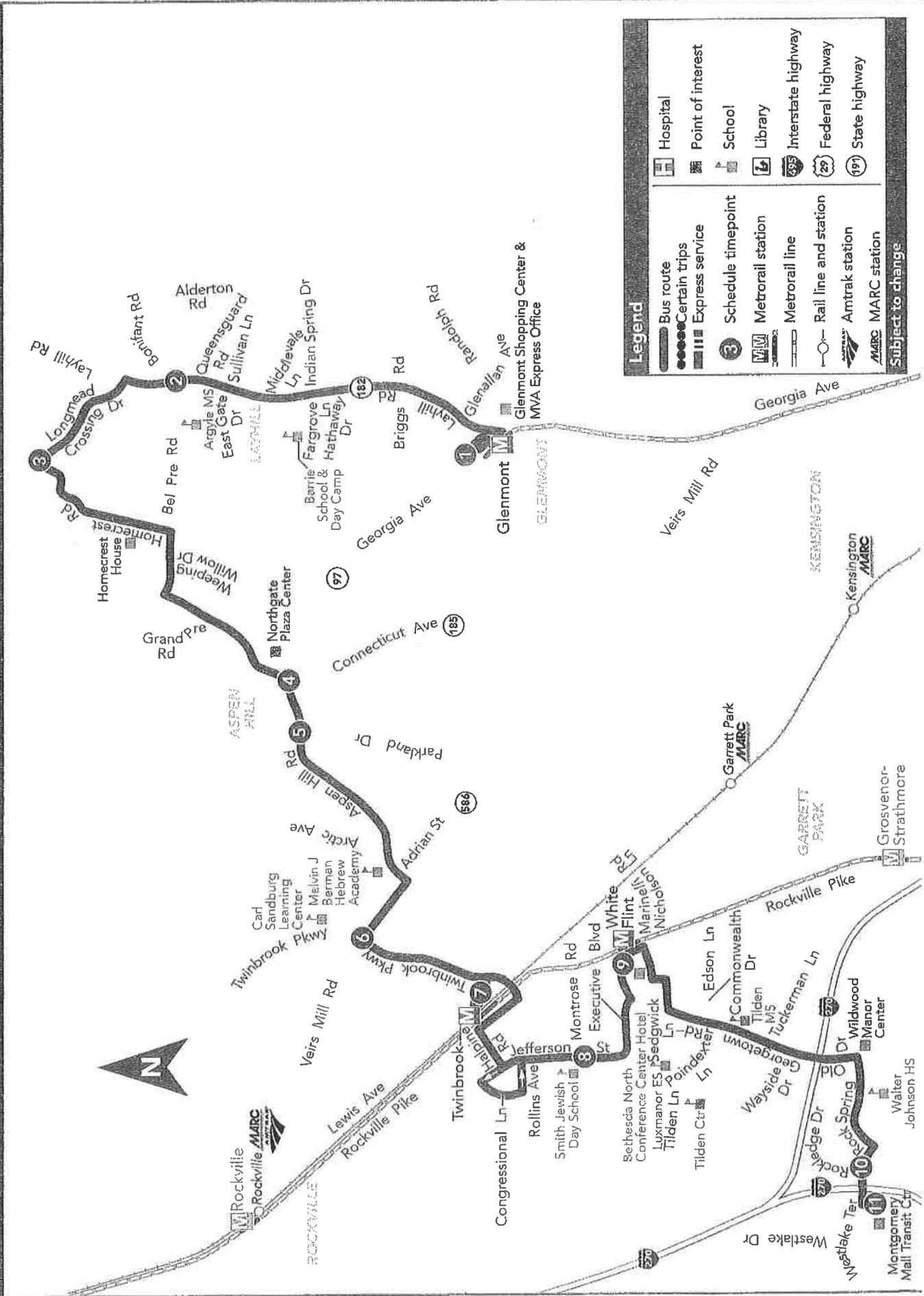
- Water
- Park
- City Limit
- Roadway
- Rail Line



Map prepared by Toole Design Group
February 2004



Glenmont Metro Station – Aspen Hill – Wheaton Woods –
 Twinbrook Metro Station – White Flint Metro Station –
 Montgomery Mall Transit Center



Monday-Friday

Saturday Schedules | Sunday Schedules | Ride On Home Page

Montgomery Mall to Glenmont

Montgomery Mall	Rockledge-Rock Spring	White Flint Station	Jefferson-Montrose	Twinbrook Station	Twinbrook-Veirs Mill	Parkland-Aspen Hill	Connecticut-Georgia	Homecrest-Longmead	Bel Pre-Layhill	Glenmont Station
5:02a							5:02a	5:07a	5:12a	5:19a
5:19a				5:19a	5:24a	5:29a	5:32a	5:37a	5:42a	5:49a
5:27a	5:29a	5:36a	5:41a	5:49a	5:54a	5:59a	6:02a	6:07a	6:12a	6:19a
5:57a	5:59a	6:06a	6:11a	6:19a	6:24a	6:29a	6:32a	6:37a	6:42a	6:49a
6:27a	6:29a	6:36a	6:41a	6:49a	6:54a	6:59a	7:02a	7:07a	7:12a	7:19a
6:49a	6:52a	7:00a	7:05a	7:16a	7:21a	7:26a	7:30a	7:36a	7:41a	7:49a
7:19a	7:22a	7:30a	7:35a	7:46a	7:51a	7:56a	8:00a	8:06a	8:11a	8:19a
7:49a	7:52a	8:00a	8:05a	8:16a	8:21a	8:26a	8:30a	8:36a	8:41a	8:49a
8:19a	8:22a	8:30a	8:37a	8:48a	8:53a	8:58a	9:02a	9:08a	9:13a	9:21a
8:49a	8:52a	9:00a	9:07a	9:18a	9:23a	9:28a	9:32a	9:38a	9:43a	9:51a
9:19a	9:22a	9:30a	9:37a	9:48a	9:53a	9:58a	10:02a	10:08a	10:13a	10:21a
9:49a	9:52a	10:00a	10:07a	10:18a	10:23a	10:28a	10:32a	10:38a	10:43a	10:51a
10:19a	10:22a	10:30a	10:37a	10:48a	10:53a	10:58a	11:02a	11:08a	11:13a	11:21a
10:49a	10:52a	11:00a	11:07a	11:18a	11:23a	11:28a	11:32a	11:38a	11:43a	11:51a
11:19a	11:22a	11:30a	11:35a	11:46a	11:51a	11:56a	11:59a	12:05p	12:09p	12:17p
11:49a	11:52a	12:00p	12:05p	12:16p	12:21p	12:26p	12:29p	12:35p	12:39p	12:47p
12:19p	12:22p	12:30p	12:35p	12:46p	12:51p	12:56p	12:59p	1:05p	1:09p	1:17p
12:49p	12:52p	1:00p	1:05p	1:16p	1:21p	1:26p	1:29p	1:35p	1:39p	1:47p
1:19p	1:22p	1:30p	1:35p	1:46p	1:51p	1:56p	1:59p	2:05p	2:09p	2:17p
1:49p	1:52p	2:00p	2:05p	2:16p	2:21p	2:26p	2:29p	2:35p	2:39p	2:47p
2:19p	2:22p	2:30p	2:35p	2:46p	2:51p	2:56p	2:59p	3:05p	3:09p	3:17p
2:47p	2:50p	2:59p	3:07p	3:21p	3:28p	3:35p	3:39p	3:46p	3:51p	3:59p
3:12p	3:15p	3:24p	3:32p	3:46p	3:53p	4:00p	4:04p	4:11p	4:16p	4:24p
3:32p	3:35p	3:44p	3:52p	4:06p	4:13p	4:20p	4:24p	4:31p	4:36p	4:44p
3:52p	3:55p	4:04p	4:12p	4:26p	4:33p	4:40p	4:44p	4:51p	4:56p	5:04p
4:12p	4:15p	4:24p	4:32p	4:46p	4:53p	5:00p	5:04p	5:11p	5:16p	5:24p
4:32p	4:35p	4:44p	4:52p	5:06p	5:13p	5:20p	5:24p	5:31p	5:36p	5:44p
4:47p	4:50p	4:59p	5:07p	5:21p	5:28p	5:35p	5:39p	5:46p	5:51p	5:59p
5:02p	5:05p	5:14p	5:22p	5:36p	5:43p	5:50p	5:54p	6:01p	6:06p	6:14p
5:19p	5:22p	5:31p	5:39p	5:53p	6:00p	6:07p	6:11p	6:18p	6:23p	6:31p
5:39p	5:42p	5:51p	5:59p	6:13p	6:20p	6:27p	6:31p	6:38p	6:43p	6:51p
5:59p	6:02p	6:11p	6:19p	6:33p	6:40p	6:47p	6:51p	6:58p	7:03p	7:10p
6:19p	6:22p	6:30p	6:37p	6:49p	6:54p	6:59p	7:03p	7:08p	7:13p	7:20p
6:49p	6:52p	7:00p	7:07p	7:19p	7:24p	7:29p	7:33p	7:38p	7:43p	7:50p
7:19p	7:22p	7:30p	7:37p	7:49p	7:54p	7:59p	8:03p	8:08p	8:13p	8:20p
7:49p	7:52p	8:00p	8:07p	8:19p	8:24p	8:29p	8:33p	8:38p	8:43p	8:50p
8:19p	8:22p	8:30p	8:36p	8:47p	8:52p	8:57p	9:01p	9:06p	9:11p	9:18p
8:49p	8:52p	9:00p	9:06p	9:17p	9:22p	9:27p	9:31p	9:36p	9:41p	9:48p
9:19p	9:22p	9:30p	9:36p	9:47p	9:52p	9:57p	10:01p	10:06p	10:11p	10:18p
9:49p	9:52p	10:00p	10:06p	10:17p	10:22p	10:27p	10:31p	10:36p	10:41p	10:48p
10:19p	10:22p	10:30p	10:36p	10:47p	10:52p	10:57p	11:01p	11:06p	11:11p	11:18p
10:49p	10:51p	10:57p	11:03p	11:13p	11:17p	11:22p	11:25p	11:30p	11:34p	11:40p
11:34p	11:36p	11:42p	11:48p	11:58p	12:02a	12:07a	12:10a	12:15a	12:19a	12:25a
12:19a	12:21a	12:27a	12:33a	12:43a	12:47a	12:52a	12:55a	1:00a	1:04a	1:10a

For any MCDOT service request or complaint, call 311 (when dialing outside of the county: 240-777-0311) or submit via their [website](#).

For website comments or to report website problems or broken links, please [email us](#). This email address does not handle service requests or complaints.

Transit Services · Montgomery County Department of Transportation



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meters

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800



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 VEHICLE TURNING MOVEMENT COUNT - SUMMARY

STSLTD STSLTD STSLTD STSLTD STSLTD STSLTD
 Location : Montgomery County
 Date : 05/26/11
 Weather : Clear
 Entered by MN

STSLTD STSLTD STSLTD STSLTD STSLTD
 Day: Thursday
 STREET
 TRAFFIC
 STUDIES
 LTD

Intersection of: Chapman Avenue
 and: Thompson Avenue
 Counted by: LR

TIME	TRAFFIC FROM NORTH on: Chapman Ave				TRAFFIC FROM SOUTH on: Chapman Ave				TRAFFIC FROM WEST on: Thompson Ave				TRAFFIC FROM EAST on: Thompson Ave				TOTAL N+S + E+W
	RIGHT	THRU	LEFT	TOTAL	LEFT	THRU	RIGHT	TOTAL	RIGHT	THRU	LEFT	TOTAL	LEFT	THRU	RIGHT	TOTAL	
AM																	
06:30-45	1	14	0	15	3	18	0	21	0	0	0	0	0	0	0	0	36
45-00	0	21	0	21	4	20	0	24	3	0	0	3	0	0	0	0	48
07:00-15	2	36	0	38	3	39	0	42	6	1	0	7	0	0	0	0	87
15-30	1	31	0	32	12	41	1	54	1	0	0	1	0	0	0	0	87
30-45	1	40	0	41	10	47	0	57	4	1	0	5	1	0	0	1	104
45-00	2	37	0	39	5	53	1	59	5	0	0	5	0	0	0	0	103
08:00-15	4	44	0	48	11	59	0	70	6	0	0	6	0	0	0	0	124
15-30	1	48	0	49	17	79	0	96	6	0	1	7	0	0	0	0	152
30-45	2	39	0	41	19	75	1	95	7	0	0	7	0	1	1	2	145
45-00	2	50	0	52	13	65	2	80	5	1	0	6	2	0	0	2	140
09:00-15	0	47	0	47	13	63	4	80	7	0	0	7	0	0	0	0	134
15-30	0	47	0	47	12	56	0	68	7	0	1	8	0	2	0	2	125
AM 3 HOUR TOTALS	16	454	0	470	122	615	8	746	57	3	2	62	3	3	1	7	1285
1 HOUR TOTALS																	
630-730	4	102	0	108	22	118	1	141	10	1	0	11	0	0	0	0	258
645-745	4	128	0	132	29	147	1	177	14	2	0	16	1	0	0	1	326
07-08	6	144	0	150	30	180	2	212	16	2	0	18	1	0	0	1	381
715-815	8	152	0	160	38	200	2	240	16	1	0	17	1	0	0	1	418
730-830	8	169	0	177	43	238	1	282	21	1	1	23	1	0	0	1	483
745-845	9	168	0	177	52	266	2	320	24	0	1	25	0	1	1	2	524
08-09	9	181	0	190	60	278	3	341	24	1	1	26	2	1	1	4	561
815-915	5	184	0	189	62	282	7	351	25	1	1	27	2	1	1	4	571
830-930	4	183	0	187	57	259	7	323	26	1	1	28	2	3	1	6	544
PEAK HOUR 815-915	5	184	0	189	62	282	7	351	25	1	1	27	2	1	1	4	571
PM																	
04:00-15	1	66	1	68	12	76	0	88	4	1	1	6	2	1	0	3	165
15-30	0	86	0	86	17	88	0	105	5	2	0	7	0	2	1	3	201
30-45	0	64	0	64	22	102	1	125	9	0	0	9	0	0	0	0	198
45-00	0	83	0	83	15	97	1	113	8	0	0	8	0	0	0	0	204
05:00-15	0	86	0	86	10	83	1	94	13	0	1	14	0	1	0	1	195
15-30	0	85	0	85	18	113	0	131	12	0	0	12	0	0	0	0	228
30-45	3	83	1	87	16	98	1	115	13	0	0	13	3	0	1	4	219
45-00	1	77	0	78	12	110	0	122	5	0	2	7	0	0	0	0	207
06:00-15	6	89	0	95	13	85	0	98	8	0	3	11	0	0	0	0	204
15-30	2	78	1	81	10	73	1	84	8	0	2	10	2	0	1	3	178
30-45	0	58	0	58	14	72	0	86	9	0	1	10	0	0	0	0	154
45-00	2	44	1	47	8	67	0	75	10	0	0	10	1	0	1	2	134
PM 3 HOUR TOTALS	15	899	4	918	167	1064	5	1236	104	3	10	117	8	4	4	16	2287
1 HOUR TOTALS																	
04-05	1	299	1	301	66	363	2	431	26	3	1	30	2	3	1	6	768
415-515	0	319	0	319	64	370	3	437	35	2	1	38	0	3	1	4	798
430-530	0	318	0	318	65	395	3	483	42	0	1	43	0	1	0	1	825
445-545	3	337	1	341	59	391	3	453	46	0	1	47	3	1	1	5	846
05-06	4	331	1	336	56	404	2	462	43	0	3	46	3	1	1	5	849
515-615	10	334	1	345	59	406	1	466	38	0	5	43	3	0	1	4	858
530-630	12	327	2	341	51	366	2	419	34	0	7	41	5	0	2	7	808
545-645	9	302	1	312	49	340	1	390	30	0	8	38	2	0	1	3	743
06-07	10	269	2	281	45	297	1	343	35	0	6	41	3	0	2	5	670
PEAK HOUR 515-615	10	334	1	345	59	406	1	466	38	0	5	43	3	0	1	4	858

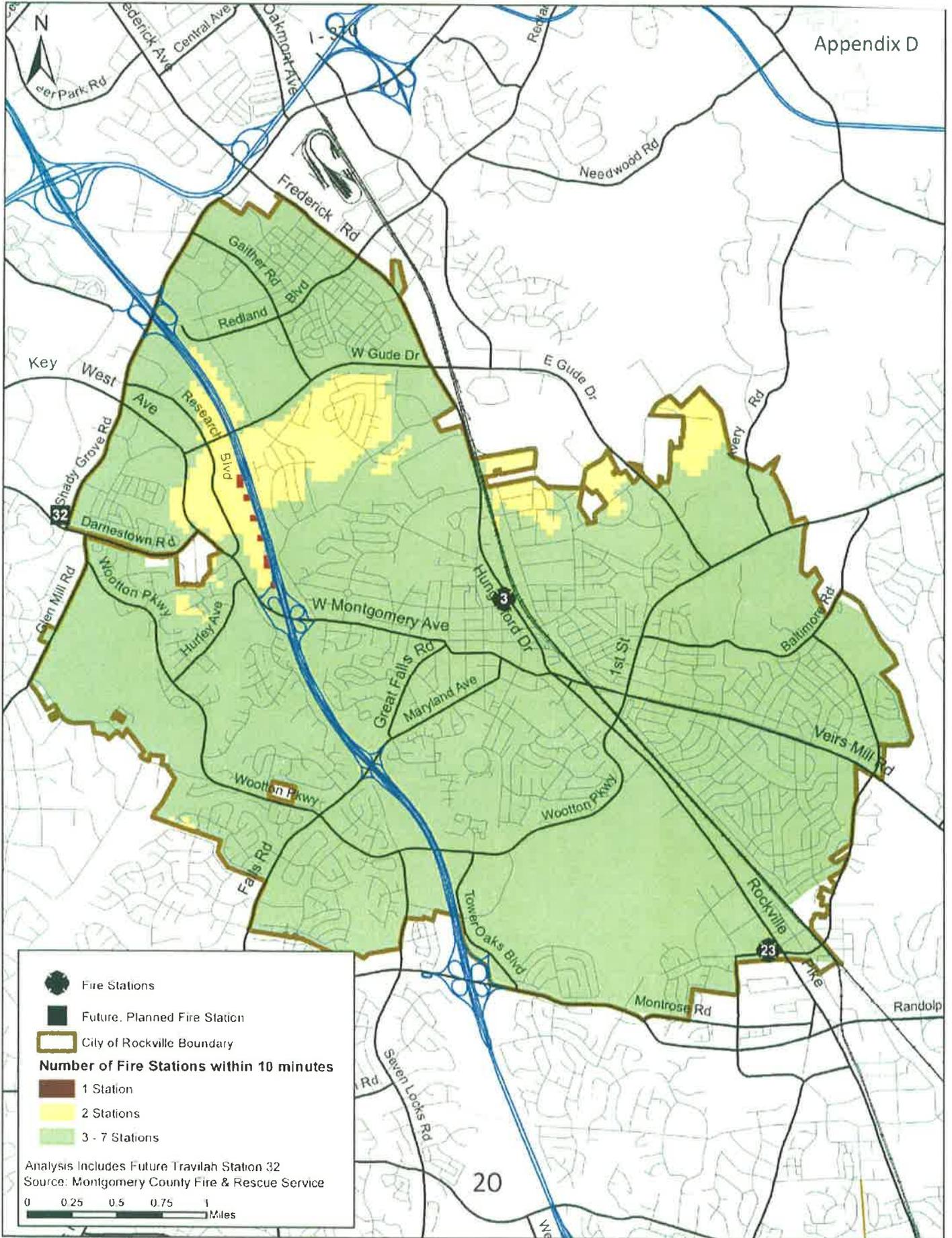
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Company Name Street Traffic Studies
 Location Chapman Avenue @ Thompson Avenue
 Date 25-May-11

Crosswalk	Hour	Chapman Ave North Leg	Chapman Ave South Leg	Thompson Ave East Leg	Thompson Ave West Leg
	06:30	0	0	0	6
	06:45	0	0	2	6
	07:00	1	1	2	9
	07:15	0	0	3	8
	07:30	0	0	2	7
	07:45	0	0	8	9
	08:00	0	0	3	4
	08:15	0	0	7	11
	08:30	0	0	4	5
	08:45	0	0	4	8
	09:00	0	3	6	4
	09:15	1	2	7	3
	14:00				
	14:15				
	14:30				
	14:45				
	15:00				
	15:15				
	15:30				
	15:45				
	16:00	2	1	4	7
	16:15	1	0	4	11
	16:30	1	2	7	18
	16:45	0	1	6	8
	17:00	2	1	16	18
	17:15	0	1	11	6
	17:30	0	0	6	12
	17:45	2	1	4	10
	18:00	1	0	5	11
	18:15	0	0	8	9
	18:30	0	0	16	7
	18:45	0	0	4	4
	TOTAL	11	13	139	199
	AM Peak Vol				
	PM Peak Vol				



STP2012-00112, 1900 Chapman Project Owner, LLC
 1900 Chapman Avenue
 July 3, 2012

The applicant's points will come from:

Points	Credit	Credit Name
1	SS Credit 1	Site Selection
5	SS Credit 2	Development Density and Community Connectivity
1	SS Credit 3	Brownfield Redevelopment
6	SS Credit 4.1	Alternative Transportation – Public Transportation Access
3	SS Credit 4.3	Alternative Transportation – Low Emission and Fuel Efficient Vehicles
1	SS Credit 5.2	Site Development, Maximize Open Space
1*	SS Credit 6.2	Stormwater Design, Quality Control
1*	SS Credit 7.1*	Heat Island Effect – Non-Roof*
1*	SS Credit 7.2*	Heat Island Effect – Roof*
2*	WE Credit 3.1*	Water Use Reduction, 30% Reduction*
1*	EA Credit 1.1*	Optimize Energy Performance, 12% new, 8% existing*
1*	EA Credit 1.2	Optimize Energy Performance, 14% new, 10% existing*
1*	EA Credit 1.3	Optimize Energy Performance, 16% new, 12% existing*
1*	EA Credit 1.4	Optimize Energy Performance, 18% new, 14% existing*
1*	MR Credit 2.1*	Construction Waste Management, Divert 50% From Disposal*
1*	MR Credit 2.2	Construction Waste Management, divert 76% From Disposal*
1	MR Credit 4.1	Recycled Content, 10% (post-consumer + ½ pre-consumer)
1	MR Credit 5.1	Regional Materials, 10% Extracted, Processed & Manufactured
1	MR Credit 5.2	Regional Materials, 20% Extracted, Processed & Manufactured
1	IEQ Credit 3.1	Construction IAQ Management Plan, During Construction
1	IEQ Credit 4.1	Low-Emitting Materials – Adhesives and Sealants
1	IEQ Credit 4.2	Low-Emitting Materials – Paints & Coatings
1	IEQ Credit 4.3	Low-Emitting Materials - Flooring Systems
1	IEQ Credit 5	Indoor Chemical and Pollutant Source Control
1	IEQ Credit 6.1	Controllability of Systems - Lighting
1	IEQ Credit 6.2	Controllability of Systems – Thermal Comfort
1	IEQ Credit 7.1	Thermal Comfort - Design
1	IEQ Credit 7.2	Thermal Comfort - Verification
1	IEQ Credit 8.1	Daylight Views, Daylight 75% of spaces
1	ID Credit 1.2	Innovation in Design, Low Mercury Lighting
1	ID Credit 1.3	Innovation in Design, Building Education
1	ID Credit 2	LEEP Accredited Professional
1	RP Credit 1.4	Regional Priority Credit: MRC2 (50%)
44	Total	*=Rockville Priority Credits (11)

Zoning Ordinance Compliance

The subject property is zoned MXTD (Mixed-Use Transit District), which allows the proposed mixture of uses. The purpose of the MXTD zone is for locations in areas near Metro stations, allowing for high-density development of retail, office, and residential uses consistent with the recommendations of the Plan. The proposed development meets the purpose and requirements of the zone as detailed below.